

REVISED
MASTER PLAN REPORT

FOR

NORTON COMMONS

LOUISVILLE, KENTUCKY

May 18, 2000

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HISTORICAL TRADITIONS,
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AND PD OPTION COMPLIANCE

Now, at the end of the 20th century, America seems to be mirroring the movement which took place in the latter part of the previous century. The 19th century was an era in which some of America's best neighborhoods were planned and developed. It also marked the creation of many wonderful parks and public spaces while still protecting many of our natural resources. That movement was started by a coalition of great planners, political and civic leaders, environmentalists and private land developers. A similar coalition is beginning today to restore our idea of sense of place for the upcoming millennium.

In the 1870s, America's urban places were mostly ugly, polluted, and unhealthy places to live. A short 20 years later, our urban places began a remarkable renaissance. The period from 1890 to 1940 marked golden years of renewal for these American cities; this renewal can trace its roots to Paris, France. In 1850, Paris was among the worst of places; so bad that various emperors, including Napoleon, didn't like to visit there. It had no sanitary sewer system; human waste was dumped out of windows into the streets. Napoleon III disliked visiting Paris so much that he directed Baron Haussmann to create a plan to let light and air in, and transform Paris into a more hospitable place.

Haussmann created a plan to take Paris far into the countryside, creating great diagonal boulevards, squares, parks, civic monuments, and public gathering places. His plan was achieved, while paying careful attention to existing buildings that often had to be refaced to front on the diagonal streets. By 1878, Paris had been transformed to a great city ready to host the World's Fair. To mark this great event, the Eiffel Tower was built. Many American leaders returned from the Fair impressed and looking forward to changing our urban areas.

In Chicago, members of a women's club, business leaders and political leadership saw the need to better plan for the future. This group agreed to hire the great planner, Daniel Burnham, an environmental landscape architect, to plan its own fair to be known as the Columbian Exposition of 1890. The same team was hired again to master plan the city and develop the Burnham Plan for Chicago, which is still in effect today. The Burnham Plan was completed at the time when Chicago already had 2,000,000 residents. Olmsted created plans for a continuous 20-mile public lakefront park connecting interior city parks through a system of greenways and a nature trail system which also connected forest preserves as far as 90 miles away. Today, some of this system is just being completed and it is possible to take nature trails all the way from Chicago to the Wisconsin State line.

The Columbian Exposition of 1890 was attended by 27.5 million people and, coupled with Burnham's Plan for Chicago, brought about the City Beautiful movement. Other cities would quickly follow by hiring this same team or others for their own beautification plans. National political leaders like Teddy Roosevelt would lead efforts for national and city parks. Big city mayors and civic leadership were working to create urban parks surrounded by village-like neighborhoods.

The City Beautiful movement was not confined to cities. In fact, planner John Nolen was creating plans for wonderful small towns like Mariemont near Cincinnati, Venice and Winter Park in Florida, Meyers Park near Charlotte, and Madison, Wisconsin. Developer George Merrick was planning the beautiful Coral Gables, Florida, while J.C. Nichols was developing Country Club Plaza in Kansas City. Others were planning such villages as Lake Forest, Illinois, and Shaker

Heights, Ohio. These towns and villages have remained some of this country's most valuable real estate.

In Louisville, too, active civic, business and political leaders were at work at the turn of the 19th century planning areas that are still great assets today. They brought Frederick Law Olmsted to town to create plans for our parks and parkways. Ultimately, the Olmsted firm would design many of our neighborhoods, including parts of the Cherokee Triangle, Anchorage, Audubon Park, Lakeside, Tyler Park, Indian Hills, and others. They also designed many of our college campuses and civic and private gardens. In fact, the Olmsted firm designed more projects in Louisville than any other city, except Boston, which was their home base. Today, the work of the Olmsted firm speaks well for the vision of our community's past leadership. They left a legacy of wonderful jewel-like places for us to live in and enjoy.

As we approach the end of this century, once again a coalition of people is leading us to recreate the kinds of communities we once had. A movement is beginning to create new neighborhoods with a real sense of place. At the forefront of this movement are Norton Commons planners, Andres Duany and Elizabeth Plater-Zyberk, of Duany Plater-Zyberk and Company. This duo has been creating plans for wonderful towns and villages throughout the world. Other planners have joined them in creating the organization known as the Congress of the New Urbanism, which seeks to reestablish America's civic realm.

Today, like the end of the 19th century, others are joining in this effort, including environmentalist organizations, political and civic leaders, planning agencies and transit authorities. Individuals and groups represented in this drive include some of the following:

- Natural Resources Defense Council
- Sierra Club
- Environmental Protection Agency
- Federal Transportation Agency
- Smart Growth (17 state governors and many mayors from both political parties)
- Vice President Al Gore
- National Town Builders Association
- Numerous cities and towns with New Urbanism growth guidelines

The development of Norton Commons has given us the opportunity to once again bring a great planning to this community and a chance to closely examine the best of our past that remains so important in the present. It is hoped that Norton Commons will do much to reestablish the civic realm and once again create many neighborhoods with a true sense of place. As developers, we face this task with a sense of our great history.

History of the Land and the Charrette Process

Norton Commons began with a decision by Mary Norton Shands and her husband, the Reverend Al Shands, and their family, in conjunction with PNC Trust. The 600-acre property which will be the site of Norton Commons has long been in their family. As a child, Mary visited the land with her father, George, an early pioneer in the television business and the owner of WAVE-TV. Mr. Norton loved farming and used the land to showcase progressive farming techniques on a

weekly television show. Thus, the property became known as the "WAVE Farm." In recent years, as developments sprung up in eastern Jefferson County, the Shands family realized that developing the farm was inevitable.

Their difficult decision was made easier when the principals of Triad Development approached them and PNC Trust with an idea of forming a partnership to develop the farm in a unique manner. Triad's proposal was to develop the farm in the village style, employing the great talents of Andres Duany, to create a wonderful place which would serve a wide range of Louisvillians. What Triad had in mind would transform the farm from a showcase for progressive agriculture to a showcase for planning and development. Duany was charged with creating a place for the ages. One that would stress healthy living, walkable neighborhoods and sensitivity to protecting the environment and character of the area.

To accomplish this task, Duany assembled a team of planners in Louisville for a 10-day design charrette. This public design process gathered input from all segments of the community including government agencies, planning officials and, most importantly, those who would be most affected by Norton Commons – neighbors of the village.

At the conclusion of the design charrette, Andres Duany made a public presentation of the plan at the Clifton Center with more than 500 people in attendance. The plan was received with great enthusiasm. On May 11, 1997, a Courier-Journal editorial said, "Mr. Duany believes we need to get back to what neighborhoods used to be; places where the old and young, the rich and poor, worked, shopped and went to school ... Certainly their ideas won't solve all the world's problems – or even all of Louisville's. But they're an exciting start."

After the charrette, many citizens worked with the Jefferson County Planning Commission and Fiscal Court commissioners to craft a Planned Village Development Ordinance which would encourage the principles of development envisioned by the Norton Commons Master Plan. These principles envision long-lasting communities and allow the vision of George Norton to continue, as he understood that land should be used wisely both in the short term and the long term. The many citizens and public officials who worked on the Norton Commons plan and the Planned Village Ordinance should be commended for their wonderful work to again create the kind of long-lasting places we have enjoyed through the vision of Frederick Law Olmsted.

In this spirit, we should remember that each of us is fortunate to live in this grand place called Louisville, lives with an undeniable truth – that each of us has been warmed by fires we did not build, quenched by waters from wells we did not dig – we should do no less for those generations which will come after us. Like those who came before us, we can accomplish so much of lasting consequence by working together as a community.

Statement of Purpose and Intent

The purpose and intent of Norton Commons is to create a traditional neighborhood development, sometimes called a "TND" by planners and architects. Two villages and a "hamlet" comprise the overall Norton Commons plan of development. This type of development is based on the creation of neighborhoods. TNDs are grounded in certain fundamental planning principles established by Andres Duany and Elizabeth Plater-Zyberk. These fundamental principles are:

1. The neighborhood has a center and an edge. The center is always a public space, which may be a square, a green or an important street intersection. Shops and workplaces are usually associated with the center. Neighborhood edges may vary in character: they can be natural, such as a forest, or manmade, such as infrastructure.

2. The optimal size of a neighborhood is a quarter mile from center to edge. This distance is the equivalent of an approximate five-minute walk at an easy pace. This limited area gathers the population of a neighborhood within walking distance of many of their daily needs, such as a convenience store, post office, community police post, automatic bank teller, school, day care center and transit stop. When an automobile trip is necessary to arrive at a transit stop, most potential users will simply continue driving to their destinations. But the TND, which focuses the required user population within walking distance of the stop, makes transit viable at densities that a suburban pattern cannot sustain.

3. The neighborhood has a balanced mix of activities – dwelling, shopping, working, schooling, worshiping and recreating. This is particularly important for those who are unable to drive – such as the young and the elderly. Additionally, the proximity of daily destinations and the convenience of transit reduces the number and length of trips, decreases the private stress of time in traffic and minimizes the public-borne expenses of road construction and atmospheric pollution. The neighborhoods will have a range of housing types for a variety of incomes, from the wealthy business owner to the school teacher and the gardener. Suburban areas, which are most commonly segregated by income, do not provide for the full range of society. The greatest contribution to affordable housing may be realized by the neighborhood's ability to reduce multiple automobile ownership and many of its associated costs. By enabling households to own one less vehicle, the average annual operating expense of \$5,000 can be applied toward an additional \$50,000 increment of mortgage financing at 10 percent.

4. The neighborhood structures building sites and traffic on a fine network of interconnecting streets.

The typical suburban traffic model finds more focus on moving traffic through a place quickly than with the quality of the place itself. Neighborhood streets of varying types are detailed to provide equitably for pedestrian comfort and for automobile movement. Slowing the automobile and increasing pedestrian activity encourages the casual meetings that form the bonds of community.

5. The neighborhood gives priority to public space and to the appropriate location of civic buildings.

Public spaces and buildings represent community identity and foster civic pride. The neighborhood plan structures its streets and blocks to create a hierarchy of public spaces and locations for public buildings. The importance of civic and community structures and space is enhanced by their suitable siting. This priority is much preferable to the standard practice of locating government and civic buildings and spaces according to the expedencies of land cost.

These principles of TND are more fully described in the Thirteen Points of Traditional Neighborhood Development on the following page.

Norton Commons offers an alternative future for the building of neighborhoods that are compact, mixed-use and pedestrian friendly; districts of appropriate location and character; and corridors that are functional and beautiful that can integrate natural environments and man-made communities into a sustainable whole. Norton Commons offers use and architectural restrictions designed to respect and incorporate Jefferson County's best and most beloved traditions of design and land use planning.*

*Much of the Statement of Purpose and Intent is excerpted or adapted from *The Neighborhood, the District and the Corridor*, by Andres Duany and Elizabeth Plater-Zyberk, pp. xvii-xx, *The New Urbanism: Toward an Architecture of Community*, Peter Katz, (McGraw-Hill, 1994).

The social and environmental benefits of a Traditional Neighborhood Development (TND) result from certain physical and organizational characteristics. An authentic Neighborhood includes most of the following:

1. The Neighborhood has a discernible center. This is often a square or green, and sometimes a busy or memorable street intersection. A transit stop would be located at this center.
2. Most of the dwellings are within a five-minute walk of the center. This distance averages one-quarter of a mile.
3. There is a variety of dwelling types within each Neighborhood. These usually take the form of houses, rowhouses, and apartments, such that younger and older people, singles and families, the poor and the wealthy, may find places to live.
4. There are shops and offices at the edge of each Neighborhood. The shops would be sufficiently varied to supply the weekly needs of a household. A convenience store is the most important among them.
5. A small ancillary building is permitted within the back yard of each house. It may be used as a rental unit or a place to work.
6. There is an elementary school close enough so that most children can walk from their dwelling. This distance would be no more than one-eighth of a mile.
7. There are small playgrounds near every dwelling. This distance would also be no more than one-eighth of a mile.
8. The streets within the Neighborhood are a connected network. This provides a variety of itineraries and disperses traffic congestion.
9. Streets are relatively narrow and are shaded by rows of trees. This slows down traffic, creating an environment for pedestrians and bicycles.
10. Buildings in the neighborhood center are placed close to the street. This creates a strong sense of place.
11. Parking lots and garage doors rarely enfront the streets. Parking is relegated to the rear of buildings, usually accessed by alleys.
12. Certain prominent sites are reserved for civic buildings. Buildings for meetings, education, religion, and culture are located at the termination of the street vistas or at neighborhood centers.
13. Neighborhoods are organized to be self-governing. A formal association debates on matters of maintenance, security and physical change.

Comprehensive Plan Guideline Compliance Statement

PNC Bank, Kentucky, Inc., Trustee under the Will of George W. Norton, Jr., Owner, and Norton Commons, LLC, Developer, are seeking to re-zone 594.76 acres from R-4 Residential to Planned Village Development district ("PVD") on property bounded generally by I-71 to the south, the Oldham County line to the north, Old Brownsboro Road (Kentucky Highway 1694) to the east, and Chamberlain Lane to the west.

The PVD Regulation itself established the internal compatibility of the mix of land uses.

Pursuant to the PVD Regulation, following is a statement of compliance with the Comprehensive Plan as required by KRS 100.213. In Jefferson County, compliance with the Comprehensive Plan is demonstrated by compliance with all applicable "Guidelines" of the Comprehensive Plan.

I. Environmental

The Norton Commons plan of development, the Master Plan and Master Plan Report (altogether, the "Plan") complies with applicable Environmental Guidelines, to wit: E-1, E-2, E-3, E-5, E-6, E-7, E-8, E-9, E-10, E-11, E-12, E-14, E-15, because: the property does not have any severe environmental limitations which cannot be mitigated, is not in the flood way of the 100-year flood plain or the fringe of the 100-year flood plain and is an area identified on Core Graphic 23 as prime vacant land for development; although there is a stream which crosses the northern portion of the site (in Oldham County), it is located in open space and no changes to the stream channel are proposed; the Norton Commons Plan is anticipated to be reviewed and approved by the Louisville and Jefferson County Metropolitan Sewer District ("MSD"), on the basis that post-development peak rates from water run-off do not exceed peak rates prior to development; the stream and lake are within open spaces such that the storm water run-off will not be a significant source of water pollution; the steep slopes on the site are located in open space and the vast majority of the site is gently rolling farm land such that there will be a minimum grading, cutting and filling activity on the site; the Developer will use the best management practices for erosion and sedimentation control during and after site preparation and construction activities as required by MSD; the stream and lake on the property will be located in open space and, therefore, are protected from construction activities and urban storm water run-off; there will be no development on sites greater than 20% slopes; development on 12% or greater slopes will be undertaken so as to ensure that the slopes will be stable; natural land forms and vegetation will be preserved to the greatest extent possible and will be in compliance with the Jefferson County Sedimentation and Erosion Control Ordinance; the development is not located in an area indicated by Core Graphic 4 as having unstable soils; the Planning Commission and Jefferson Fiscal Court should determine that this site will not be an indirect source of air pollution or cause any significant degradation of air quality and air pollution resulting from construction and demolition activities will be insignificant because there will be few demolitions and minimal grading and cutting activity, and best management practices will be utilized, and no construction will occur on soils with severe erosion problems (Core Graphic 3), unstable soils (Core Graphic 4), or wet soils (Core Graphic 5).

II. Utilities

The Plan complies with all applicable Utilities Guidelines including Utilities Guidelines U-1, U-2, U-3 and U-6 because: the area is currently served or will be served in the immediate future by water, sewer, gas and electric utilities; this site has an adequate supply of potable water and water for fire fighting purposes and will be served by the MSD Hite Creek waste water treatment plant from a series of pump stations throughout the

development; the plan will be reviewed by the Jefferson County Health Department in the TRC process; utility easements are designed to provide access in common areas or along lot lines and will be buried to minimize negative visual impacts.

III. Transportation

The Plan complies with applicable Transportation Guidelines, to wit, T-1, T-2, T-3, T-4, T-5, T-6, T-7, T-8, T-9, and T-11 because: the village concept will reduce automobile dependency and vehicular miles traveled; residences will be within an approximate 5-minute walk to Village Centers; the Plan provides a hierarchy of thoroughfares, connectors, local access streets and alleys designed and built to County standards which provide walkways for pedestrians as well as bicycle routes (the streets of the Village) which connect the Village Centers to the Village General and the Village Edge and hamlet areas of the development such that residential neighborhoods, recreation areas, and commercial and employment centers are accessible by all modes of transportation; all uses will be provided with parking pursuant to the minimum parking requirements of the PVD Regulation; parking will be concentrated in public parking lots in the Village Center to create a pedestrian atmosphere; two parking spaces for each principal dwelling and one space for each accessory dwelling will be provided in the residential areas; one parking space for each 400 square feet of gross office space will be provided for in the office portion; and one parking space for each 250 square feet of gross retail space will be provided for commercial uses in the Village Center; adequate parking will be provided for in civic areas shown on the Master Plan; access to the greater intensity in the Village Center will be provided via the thoroughfares so as not to create significant nuisances for the surrounding residential development; and transit shelters are also provided for public transportation.

IV. Residential

The Plan complies with all Guidelines for Residential Uses, to wit: R-1, R-4, R-5, R-6, R-7, R-8, R-9, R-10, R-11, R-13, R-14, R-15, and R-16, because: the proposed development is located between two large subdivisions (Wolf Trace and Glen Oaks), and open space buffers are provided around the perimeter of the site. Sufficient transitions are provided to make the Village compatible in height, size mass and scale (this portion of the County has experienced rapid growth in residential, office and commercial uses). Structures in each of the Village components will be visually restricted so they are not significantly different in height, size, mass or scale from adjacent developments, and are compatible with adjacent land uses; there are adequate storm and sanitary sewage facilities and potable water, as well as a lack of environmental constraints on the site; there are seven major access points, fire protection, schools and public transit facilities to permit the location of high density residential in this location; the site is not within the 40-Year Noise Exposure Forecast ("NEF") of Louisville International Airport, nor is it in the 100-year flood plain; is a mixed-use planned community that utilizes cost-efficient site layout and design techniques creating new self-contained neighborhoods and areas with adequate lot sizes, usable open spaces, landscaping and other site amenities that minimize disruption of the natural site in providing for recreational and pedestrian needs while minimizing traffic hazards; there will be a range of housing styles and affordability. For example, home prices will range from \$80,000 to \$1,000,000. The proposed development is compatible with adjacent properties along Chamberlain Lane and Old Brownsboro Road (Kentucky Highway 1694) because of the separation distance provided by the roadways and open spaces provided at the development edge; additionally, larger lots of Norton Commons abut the Hook property to the south and large open areas exist to the north of the hamlet in Oldham County.

V. Commercial

The Plan complies with all applicable Commercial Guidelines relating to commercial development, to wit, C-1, C-2, C-3, C-5, C-6 and C-7 because: the village concept will locate commercial uses at the core which will be central for the intended service area surrounded by a sufficient support population in the office and residential uses which transition from high density to lower density in the Village Edge and hamlet areas; the Plan includes sidewalks and bicycle paths constructed with the Americans with Disabilities Act ("ADA") standards; streets and public areas will be landscaped and tree-lined; the Village Center will include retail shops and service establishments as well as civic, general and medical office uses along with higher density residential uses that will transition to lower density residential uses at the outer edges of the development; the Master Plan includes a sign code to prevent visual nuisances and safety hazards to vehicular traffic; the transition of uses as well as landscaping for buffering and screening will mitigate potential nuisances that commercial uses may inflict on residential uses; commercial uses will be in a proposed planned commercial center (the Village Center) which will primarily serve residents of the development in an appropriately scaled design and character; commercial uses will serve small areas of the adjacent neighborhoods to provide convenience goods with safe pedestrian access in a good transition between adjacent uses.

VI. Office Space

The Plan complies with the Guidelines of the Comprehensive Plan regarding office space, including O-1, O-3, and O-5 because: the proposed 360,000 square feet of gross leaseable office space is provided in the context of a mixed-use planned community with adequate parking and roadway facilities to prevent nuisances and with a size, intensity and character that will be compatible with adjacent areas; offices will be mixed with residential, commercial, retail and service establishments and civic uses with planned circulation patterns for pedestrians, bicycles and persons with disabilities; with landscaping and amenities that will provide a good transition between adjacent building and land use in terms of building size, height, scale and materials; and all signage will adhere to a sign code to provide a consistent scheme in the development.

VII. Community Facilities

The Plan complies with the applicable Guidelines relating to Community Facilities, including F-1, F-2, F-3, F-4, F-5, F-6, F-8, F-11, F-14, and F-19, because: the planned community will concentrate residential, commercial and office uses in order to be sustainable, with convenient access to the hierarchy of roadways with sidewalks, bike paths, and amenities designed to ensure accessibility to all persons, including accessibility to the elderly and persons with disabilities; civic uses will be located throughout the Village to prevent potential adverse impacts on surrounding land uses; civic uses will be planned in a way so as to be compatible with other facilities; facilities will be designed so that exteriors of the structures will not detract from the residential character in the immediate neighborhood and a scale and design that will continue to serve their intended functions; adequate fire protection will be provided by the Worthington Fire Department; 155 acres of open space is provided in a series of parks dispersed throughout the development; medical offices will be located with good access to the thoroughfare system to provide quick access to regional hospital facilities; and human services facilities will be located at highly accessible locations in the Village Center.

VIII. Government

The Plan complies with all applicable Government Guidelines, including G-1 and G-4 because: essential services are provided to the site without any investment in additional public facilities and housing; commercial opportunities will be accessible regardless of age, sex, race, color, creed, national origin, income, religion, handicap or political affiliation; and because of proposed right-of-way dedications along Chamberlain Lane and Old Brownsboro Road (Kentucky Highway 1694).

The Norton Commons Master Plan complies with the requirements and guidelines of the PVD Regulation (referred to in the PVD Regulation Sec. 4 (a) as the "PD Option"). Norton Commons consists of two Villages and a hamlet.

Village Pattern. Norton Commons has each of the required components: Village Edge, Village General, and Village Center.

Required Components

- The Village Edge of Norton Commons is the least dense component, having open space and the largest (least dense) single family lots. The Village Edge has large open space areas, including parks and community gardens, with civic use locations shown on the Master Plan.
- The Village General is primarily residential, but allows certain residential uses, such as semi-detached dwellings, attached dwellings, two-family dwellings and multi-family dwellings that are not permitted in Village Edge. Certain other uses are permitted in the Village General that are not permitted in the Village Edge.
- The Village Center allows higher density development – as compared to the other two components – and serves as the focal point and primary gathering place of the Village. Uses permitted in the Village Center include village-serving shops and services, civic and office buildings, and more formalized open space.

Required Features

- All points within each Norton Commons Village are located within an approximate five-minute walk from the Village Center.
- A variety of housing types is permitted in Norton Commons as shown on **Urban Standards: Illustrated**. The types of housing which are permitted are villa, house, cottage, single house, patio house, apartment building, rowhouse and flexhouse. Live/work units are permitted. **Frontage Standards** are also proposed, and are located in the Master Plan Report.
- Streets are interconnected; cul-de-sacs are not shown on the Master Plan and are not permitted. The types of streets to be constructed in Norton Commons are those shown on the **Thoroughfare Standards**. These streets will be tree lined.
- Designated sites for civic uses are shown on the Master Plan. These locations are prominent locations within Norton Commons and several civic sites are located in open space areas and will be planned in coordination with these open spaces.
- Buildings shall be human-scaled. Building heights and setbacks vary depending upon whether the building is located in Village Edge, Village General or Village Center. These differentiations are shown on the **Site Design Standards**. Civic buildings shall have one or more distinctive features that will differentiate and elevate their role from that of private or other structures.

- Open spaces shall be permanently dedicated and are intended to preserve natural features including significant tree stands and sloped areas. A network of trails forms a link around the perimeter of the entire development, allowing for pedestrian or bicycle access into all areas.
- Cultural resources, other than otherwise addressed in this Statement will be preserved and re-used.

Development Standards.

- **Size of Site.** The overall development acreage is 594.7 acres, developed in two Villages and a hamlet. The hamlet is to be located in Oldham County. A hamlet has a Village Edge and a Village General but no Village Center. These components are integrated into the overall Norton Commons development and all areas within Jefferson County are subject to the requirements of the PVD Regulation and all development approvals granted in this Application.
- **Density.** The overall residential density of Norton Commons is 4.85 dwelling units per acre, as shown on the Master Plan.
- **Village Edge.** The Village Edge comprises a large portion of the Village as can be readily seen on the Master Plan. The Village Edge establishes a transitional area between Residential District R-4 subdivisions and nearby farms and low density single-family areas by inclusion of permanent open spaces and large single-family residential lots. Additional plantings will be provided wherever needed to provide appropriate transitions and adequate screening.
- **Village Center.** As stated above, all points within Norton Commons are within an approximate 5-minute walk of a Village Center. At least 10% (288 dwellings) of the total 2,880 residential dwellings proposed in Norton Commons are to be located within the Village Center. Including Oldham County, 17% of Norton Commons' gross acreage is located in the Village Center. Excluding Oldham County, 23% of Norton Commons' gross acreage is located in the Village Center. 200,000 sq. ft of gross floor area is proposed for retail use in the Village Center, and 360,000 sq. ft of gross floor area is proposed for office and service use in the Village Center.
- **Open Space.** Open Space is 26% of the entire Norton Commons development. At least 70% of the required Open Space will be publicly-accessible and designed for outdoor recreation. Open Space is located in each of the components of Norton Commons.
 - Village Edge. The Open Space will generally become a system of community parks that will be operated and maintained by the Norton Commons Community Association, as opposed to setting aside areas as conservation easements. However, conservation easements may be utilized as the need arises.
 - Village General. In the Village General, Open Space shall be designed as squares, greens, sports fields, pedestrian or bicycle trails, or greenways. Each Village General lot shall be located within 1,350 of an Open Space area.

- Village Center. Open Spaces shall be squares, plazas and greens.
- More than 50% of the Open Space in Norton Commons faces a public thoroughfare.
- All land used to meet the Open Space requirements as shown on the Master Plan are to be publicly accessible, with the potential exception of lands used for natural resource protection, land managed for the production of resources such as garden produce, and lands used for public health and safety purposes. It is not anticipated at this time that conservation easements will be utilized in Norton Commons, but they may be utilized in the future. Any conservation easement land may not be publicly accessible.
- **Civic Uses.** Thirteen acres of Civic Use shall be developed which is 2% of the Norton Commons development.
- **Street Network**
 - Both the Master Plan and the Thoroughfare Standards of the Master Plan Report designate the hierarchy of streets in Norton Commons. All streets will have curbs, on-street parking, street furniture – such as benches, rest stops, water fountains and sidewalks. The streets of Norton Commons shall also be utilized for bicycle movement.
 - Streets, alleys and bicycle/pedestrian pathways (trails) and sidewalks are shown on the Master Plan. All are interconnected within Norton Commons and connect to thoroughfares outside the development. No cul-de-sacs are proposed.
 - Street widths are more narrow than otherwise provided for in the Metropolitan Subdivision Regulations. This narrowness causes reduced vehicular speeds. The frequency of stop signs throughout the development will additionally reduce traffic speed, promoting pedestrian safety.
 - Alleys are to be located to the rear of structures in the Village Center, Village General and Village Edge, as shown on the Master Plan.
 - Transit shelter locations in the Village Center are shown on the Master Plan, as required.
 - Sidewalks and/or bicycle/pedestrian pathways are provided along each street or in Open Space areas.
 - Tree strips are shown on the Master Plan, the *Thoroughfare Standards* and the *Landscape Standards* of the Master Plan Report.
- **Blocks.** Blocks vary in size as shown on the Master Plan. No block is longer than 1,000 feet. Block shape and size preserve natural topography, existing vegetation, and hydrology.
- **Storm Water Management.** Norton Commons is designed to meet the MSD and Comprehensive Plan requirement that post-construction run-off water not exceed pre-construction rates.

- **Utility Services.** Louisville Water Company service will be utilized by Norton Commons to provide for adequate fire protection, an approved public water supply and an approved sanitary sewer system, all in accordance with MSD and Jefferson County Division of Environmental Health and Protection requirements.

Land Use. Land uses that are permitted, limited or prohibited are shown on the *Table of Permitted, Limited and Prohibited Uses* in the Master Plan Report. Uses not shown on the Table may be permitted as conditional uses, as set forth in PVD Regulation Sec. 2.1.5.

Site Design. The Master Plan Report contains the following standards for site design: *Landscape Standards, Environmental Standards, Use Standards, and Urban Standards: Illustrated*. These site design standards apply to all sites, including Civic Use sites.

Architectural Design. *Architectural Design Standards* are located in the Master Plan Report. These Standards encourage architectural compatibility between structures, human-scale design, pedestrian use of Norton Commons, a relationship to street and surrounding buildings. Civic buildings shall incorporate one or more distinctive features to recognize their civic function.

Parking and Loading. Norton Commons will comply with all Parking and Loading (PVD Regulation Sec. 2.1.8) requirements.

Signs. Signage will comply with PVD Regulation Sec. 2.1.9, as more fully shown on the *Sign Standards* and *Architectural Design Standards*, both located in the Master Plan Report.

Landscape. Street tree requirements and parking lot tree requirements (screening) are provided for in the *Landscape Standards* located in the Master Plan Report.

For the foregoing reasons, Norton Commons complies with the Guidelines of the PVD Regulation.

MASTER PLAN MAP AND STATUS OF OTHER MAPS

Master Plan Map

The Louisville and Jefferson County Planned Village Development Ordinance requires approval of a Master Plan Map that depicts a variety of information about the Norton Commons project. The approved Master Plan Map is included with this Master Plan Report.

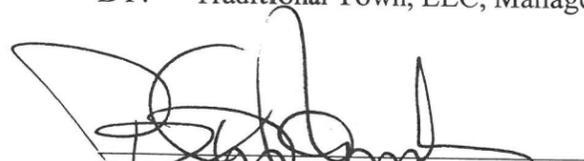
Status of Other Maps

Other maps are included in this Master Plan Report as a means of illustrating existing conditions, and conceptual plans for stormwater management and sanitary sewer service. These maps are not intended to have the same status under the Planned Village Development Ordinance, particularly as related to changes that may occur to these conceptual plans during the design and permitting process. Therefore, amendment of these maps and plans shall not constitute a major amendment pursuant to the provisions of Section 3.2.2 of the PVD ordinance.

Binding Commitments

Succeeding sections of this Master Plan Report contain a variety of "commitments" relative to the Norton Commons PVD. Each of these commitments shall be binding upon the Norton Commons Developer, and its successors and assigns, unless amended pursuant to the provisions of the Planned Village Development Ordinance. These "commitments," or conditions of development, are intended to be integral to the master plan and have the same purpose as binding elements under the "plan certain" provisions of the Land Development Code. The Norton Commons developer agrees with each and every commitment stated herein and has affixed his signature in the space provided below to memorialize this agreement.

NORTON COMMONS, LLC
BY: Traditional Town, LLC, Manager


Rodney Henderson, Managing Director

May 18, 2000
Date

NORTON COMMONS, LLC
BY: PNC Bank, Kentucky, Inc. as Trustee
Under the Will of George W. Norton, Jr.


Tawana Edwards, Sr. Vice President

May 18, 2000
Date

DESCRIPTION OF THE MIX OF LAND
USES AND THE FACTORS WHICH
ENSURE COMPATIBILITY BOTH
WITHIN THE DEVELOPMENT SITE
AND WITH ADJACENT LAND USES

The array of housing and commercial building choices (including flexspace and office buildings) is shown on *Urban Standards: Illustrated*.

Except for those uses designated as Prohibited in the *Table of Permitted, Limited and Prohibited Uses*, land uses proposed in Norton Commons are all those permitted in the Planned Village District ("PVD") Regulation. Under the PVD, less intense uses such as larger lot, single-family residential and civic open spaces are to be located in Village Edge. Village General is primarily residential, but allows a mix of other uses at limited locations. Village General also contains permanent open space. Village Center is the focal point and primary gathering place of the village. It is characterized by higher density residential, village-serving shops and services, civic buildings and more formalized open spaces such as plazas and squares. The PVD Regulation itself establishes the internal compatibility of the mix of land uses.

Norton Commons will provide a variety of housing opportunities and costs so that its neighborhoods offer living opportunities to the wealthy business owner wishing to live on a larger lot, but also to the school teacher and gardener. Affordable housing choices include garage apartments in conjunction with single-family houses, apartments above shops, and apartment buildings adjacent to shopping and workplaces.

In each of these three basic components of the Norton Commons development (Village Edge, Village General, and Village Center) permitted and conditional uses are allowed only as directed by the PVD Regulation. In order to ensure compatibility of building design, Norton Commons Community Association documents will establish architectural and design controls that will be enforced by the Town Architect.

In Oldham County, Norton Commons proposes a hamlet. A hamlet has a Village Edge and a Village General, but no Village Center.

The Norton Commons Master Plan demonstrates compatibility with adjacent land uses. The development has very large areas of Village Edge. Along Chamberlain Lane, Norton Commons lies adjacent to subdivision development (Wolf Trace), farms and open spaces. The Master Plan shows that along Chamberlain Lane these off-site neighboring properties will be protected by Park A, Park B, and larger lot single-family residential uses. Along Old Brownsboro Road (Kentucky Highway 1694), Norton Commons lies adjacent to subdivision development (Glen Oaks), farms and open spaces. The Master Plan once again shows that along its eastern property boundary lies Park J, Park H and Community Gardens, Park G, Recreation Area B, Park F, Park E and Community Gardens, Park D and Park C. The property to the southwest of the Development Site owned by Susan E. Burkley will abut larger single-family lots and Park L. The open space areas typically have trails.

It is anticipated that the proposal will receive all transportation approvals required by governmental agencies, including the Jefferson County Department of Public Works, the Kentucky Department of Transportation, and the Jefferson County Air Pollution Control District. These approvals, when secured, will demonstrate that (a) the build-out of Norton Commons will not adversely impact the traffic-carrying capacity of the adjacent street network, (b) internal street patterns will disperse and "calm" traffic, and (c) will not serve as a cause of significant air quality degradation. All streets will be public streets and all alleys will be privately-owned and maintained by the Norton Commons Community Association.

STATISTICAL INFORMATION

STATISTICAL INFORMATION.

1. Gross Acreage.

Gross Acreage	594.76 ¹
Net Acreage	589.00 ²

2. Maximum Number of Dwelling Units Proposed. 2,880³

**3. Maximum Amount of Retail, Office and Service
Uses Requested in Village Center (Gross Leaseable Area).**

200,000 SF – Commercial
360,000 SF – Office and Service Use

4. Land Devoted to Open Space.

155 Acres
89 Acres – Jefferson County
66 Acres – Oldham County

Open Space is 26% of the development including both Jefferson and Oldham Counties.

5. Land Devoted to Civic Uses.

13 Acres

Civic Use is 2% of the development including both Jefferson and Oldham Counties.

¹Including Oldham County property.

²Same as Footnote 1.

³A minimum of 10% (288) housing units to be located in Village Center.

The community has been an active participant in the planning and design of Norton Commons and it is the intent of the Developer to continue this involvement. This active participation is best represented by the Charrette Study, which documents the ongoing process of planning and community involvement. This Study is included to illustrate planning and design concepts that have been considered in formulating this Master Plan Report. Also, developing a strong civic element of the project as well as the social fabric of "community" is as important as building a connected street system. To accomplish these objectives the following commitments are made.

Commitments

The Norton Commons developer shall create and organize a **Citizens Advisory Committee ("CAC")** for the Norton Commons project. The purpose of this committee shall be to provide input and direction to the Developer for the ongoing planning, design and implementation of the project. At such time as the Norton Commons Community Association becomes active and operational, the responsibilities of the CAC will transfer to the Norton Commons Community Association. However, to maintain continuity between the functioning of these organizations, the non-resident named members of the CAC shall become ex-officio members of the Norton Commons Community Association's Executive Committee (although ex-officio membership by non-residents shall not obligate any non-resident ex-officio member to any expense, fee, cost or other obligation or liability incurred by the Norton Commons Community Association).

The CAC shall be organized within 90 days of final project approval and shall include a designated representative of: the Wolf Pen Preservation Association, Inc., a designated representative of the Glen Oaks Neighborhood Association, or any successor associations or organizations of either association, and a resident of Mint Springs Branch Road (whether or not a member of Wolf Pen Preservation Association, Inc.). A representative of the Norton Commons developer shall permanently chair the Committee and provide technical and clerical support to the operation of the Citizens Advisory Committee. The Citizens Advisory Committee shall, at the time of its organization, determine its scope and operational guidelines. The scope shall include, at a minimum, review of construction, erosion control, stormwater management and sanitary sewer plans prior to approval and subsequent permitting.

In addition to creating a Citizens Advisory Committee, the Norton Commons developer commits to an active role in seeking and securing civic uses to locate within the project. Such uses may include churches, temples, synagogues, libraries, post offices, fire and police stations, and public or private educational facilities.

Any proposed minor revision to the Master Plan or Master Plan Report shall be submitted to the Citizens Advisory Committee at least 14 days prior to submission to the Director. The Citizens Advisory Committee shall have up to five days to review the revisions and submit to the Developer requested changes.

Any proposed major revision to the Master Plan or Master Plan Report shall be submitted to the Citizens Advisory Committee at least 48 hours prior to submission to the Planning Commission. The Citizens Advisory Committee shall have up to 48 hours to review the revisions and submit to the Developer requested changes.

The Norton Commons Developer will submit organizational documents (Covenants, Restrictions and Duties) concurrent with the first Final Plan. The organizational documents (Covenants, Restrictions and Duties) will include, at a minimum, provisions for implementation of architectural standards.

PLAN FOR PEDESTRIAN,
BIKEWAY AND VEHICULAR CIRCULATION
DESCRIBING THE GENERAL DESIGN CAPACITY, AS WELL
AS ACCESS POINTS TO THE MAJOR THOROUGHFARE SYSTEM

Pedestrian and Bicycle System Commitments

Sidewalks will be constructed as shown on the street classification diagram included with the Master Plan Map and also shown on the **Thoroughfare Standards**. Each street except Street Classification A-1 will have sidewalks on either side of the travel lane of the roadway. Bicycles will utilize the public streets. Bicycle parking facilities will be provided at bicycle destination locations, such as shops and civic buildings, schools and parks. Combined pedestrian and bike paths will be provided through the open space areas as shown on the Master Plan.

The Norton Commons developer will also commit to working with the Louisville Bicycle Club to further refine plans for bicycle routes and facilities within the project. This effort will be conducted during the first 180 days following final approval of the Master Plan and PVD Rezoning. The objectives of this cooperative effort will include: determining how bicycle traffic will traverse the Norton Commons site before and after the closure of Chamberlain Lane; the criteria to be used in the design, placement and signage of on-street and off-street bicycle and/or pedestrian facilities; and a general plan of how the internal bicycle and pedestrian circulation system will be phased. The work product of this cooperative effort will be included as part of the project design guidelines that will be implemented through the review and actions of the Developer, Village Architect and Citizens Advisory Committee.

Vehicular Circulation Commitments

The vehicular circulation system for Norton Commons has two components. The first includes streets that are located either on the site or immediately adjacent to the site. The second addresses those streets and thoroughfares that are not located immediately adjacent to the site but provide access to and from the development. Vehicular circulation commitments are discussed for each of these components.

On-Site or Near Site Commitments

Vehicular circulation is shown on the Master Plan Map and has been designed to efficiently handle the anticipated traffic of the Village. No cul-de-sacs are planned and all streets are interconnected providing a method to disperse traffic flows.

Vehicular access points to Chamberlain Lane shall occur at the following intersections (with Chamberlain Lane): (1) Street A, and (2) Street S. The intersection of Street A and Wolf Pen Branch Road will be signed to encourage traffic to utilize Chamberlain Lane for access to KY 22, and to discourage traffic from using Wolf Pen Branch Road as a principal means of access to U.S. 42.

Vehicular access points to Old Brownsboro Road (Kentucky Highway 1694) shall occur at the following intersections (with Kentucky Highway 1694): (1) Street A, (2) Street EE, (3) Street AA, (4) Street BB, (5) Street JJ, (6) the southerly portion of Street B [Street B is a "horseshoe-shaped" roadway intersecting Kentucky Route 1694 at two locations] and (7) the northerly portion of Street B.

Prior to the issuance of certificates of occupancy (a) Developer shall dedicate 40 feet of right-of-way from the centerline of Old Brownsboro Road (Kentucky Highway 1694); and (b) Developer shall reconstruct a three-lane section of Kentucky Highway 1694, as directed by the Kentucky Department

of Highways, between the I-71 bridge and Street A. The three-lane section shall be appropriately transitioned to the two-lane bridge that spans I-71. Compliance with this commitment shall entitle the Developer to construct and occupy 70,000 gross square feet of commercial space, 125,000 square feet of civic and office space, and 880 residential dwelling units.

Prior to the issuance of certificates of occupancy for additional development anticipated by this paragraph, Developer shall reconstruct an additional three-lane section to Kentucky Highway 1694 as directed by the Kentucky Department of Highways from Street A to Street EE, the completion of which shall entitle Developer to construct and occupy 155,000 gross square feet of commercial uses, 260,000 square feet of office and civic uses, and 1,850 dwelling units.

Prior to the issuance of certificates of occupancy for additional development anticipated by this paragraph, Developer shall reconstruct an additional three-lane section to Kentucky Highway 1694 as directed by the Kentucky Department of Highways from Street EE to Street JJ with appropriate transition to a two-lane section north of Street JJ, the completion of which shall entitle Developer to construct and occupy the remaining approved square footages of commercial, civic and office use and remaining approved residential dwelling units as depicted on the Master Plan Map.

Chamberlain Lane

Prior the issuance of certificates of occupancy for the first traffic phase of development (a) Developer shall dedicate sufficient land from the centerline of the existing roadway to constitute a 35 foot right-of-way; and (b) Developer shall reconstruct a three-lane section of Chamberlain Lane between the I-71 underpass and Wolf Pen Branch Road/Street A. The three-lane section shall be appropriately transitioned to the two-lane underpass to I-71. Compliance with this commitment shall entitle the Developer to construct the all of the development program depicted on the Master Plan Map.

The portion of Chamberlain Lane north of the intersection of Street A and south of Mint Springs Branch Road shall be improved to Jefferson County standards for a two lane local road and the Developer will dedicate sufficient right-of-way to accomplish any required improvements. This improvement shall be completed concurrently with either the connection of Street S to Chamberlain Lane or the closure or discontinuation of Chamberlain Lane east of Mint Springs Branch Road. If the Norton Commons developer completes this improvement prior to the completion of improvements for Chamberlain Lane required of the Wolf Trace subdivision, then Jefferson County shall direct the developer of the Wolf Trace Subdivision to reimburse the Norton Commons developer for improvement costs to the extent of the Wolf Trace requirement. Jefferson County shall be the sole arbiter of the amount of the reimbursement.

Following construction and acceptance by Jefferson County of the first roadway connection between Chamberlain Lane and Kentucky Highway 1694 Developer shall be permitted to apply for the closure of that portion of Old Chamberlain Lane between Mint Springs Branch Road and Kentucky Highway 1694, or this portion of Old Chamberlain Lane shall be permitted to be discontinued pursuant to KRS Chapter 178. The Developer will work with the Louisville Bicycle Club to ensure that when this portion of Chamberlain Lane is closed or discontinued that the first street connection between Chamberlain Lane and Kentucky Highway 1694 is appropriately signed to permit the safe movement of bicycle traffic.

Off-Site Commitments

The Norton Commons developer will financially participate in the planning, design and/or construction of a series of off-site improvements to Kentucky 22 and its intersections with I-265, Chamberlain Lane and Kentucky Highway 1694. The total dollar value of this commitment shall be no more than \$700,000 and shall represent the entire responsibility of the Norton Commons developer for off-site impacts related to the development program depicted on the Master Plan Map. The Kentucky Transportation Cabinet and/or Jefferson County may request completion of this commitment by one of the following actions.

1. The Kentucky Transportation Cabinet (KTC) and/or Jefferson County may request, upon written notice to the Developer, a lump sum payment of the \$700,000. This request shall not be made any earlier than issuance of the first certificate of occupancy for a residential unit within Norton Commons. The Norton Commons developer shall submit full payment within 30 days of receipt of the notice. Furthermore, KTC and/or Jefferson County shall apply these funds toward the planning, design and construction of improvements to KY 22 as contemplated by these commitments.

-OR-

2. The KTC and/or Jefferson County may request, upon written notice to the Developer, that it initiate and complete improvements to the intersections of KY 22 with Chamberlain Lane and Kentucky Highway 1694. These improvements shall include the construction of dual left turn lanes from KY 22 to westbound Chamberlain Lane and 1694 and signalization of the intersections. Additional improvements will include the three-lane reconstruction of Chamberlain Lane and KY 1694 from KY 22 to the I-71 underpass or overpass that is possible within the existing right-of-way, or right-of-way that may be made available by Jefferson County. If this alternative is selected, the Developer shall be permitted to complete the required improvements within two years of receipt of notice.

-OR-

3. The KTC and/or Jefferson County may request, upon written notice to the Developer, that it initiate and expend all or part of its required commitment of \$700,000 to the planning and design of any or all of the following improvements to KY 22 and its intersections: the five-lane reconstruction of KY 22 from Chamberlain Lane to Kentucky Highway 1694; the improvement of exit ramps eastbound from I-265 to north and southbound KY 22; three-lane improvements to Chamberlain Lane and Kentucky Highway 1694 between KY 22 and I-71; and intersection improvements and signalization. In the event this alternative is selected the requesting agency and the Developer shall agree to a schedule for the completion of the tasks that are the responsibility of the Developer.

Air Quality Commitments

The Norton Commons developer has completed an analysis of air quality impacts of the first traffic phase of development that has been accepted by the Jefferson County Air Pollution Control District (APCD) and determined to be accurate in terms of its findings and conclusions.

Prior to TRC approval of the first Final Site Plan for Traffic Phase II and Traffic Phase III, the applicant/developer shall submit an updated traffic report and air quality analysis, and obtain approval of same from the Jefferson County Air Pollution Control District and the Jefferson County Department of Public Works. This approval is required for the purpose of a full assessment of the traffic and air quality impacts and any mitigation that may be required of the proposed development for either Traffic Phase II or III, or any portion thereof.

Transit Commitments

The Norton Commons Developer commits to accomplish the following transit related actions as a means of encouraging the availability of transit service for residents, employees, shoppers and students. First, the Developer will work with the Transit Authority of River City (TARC) to plan bus service to the project at the appropriate time, as well as circulator service to nearby shopping and workplace opportunities. Transit stops and shelters will be incorporated in the plan of development, particularly within the Village Centers.

Internal Circulation Commitments

The Norton Commons Developer will commit to including an additional internal street connection whose purposes would be: to provide a second connection between the two villages; and, provide an alternative method of moving traffic from the northernmost village to Chamberlain Lane. This connection will traverse the open space area that generally divides the villages in the vicinity of Street J, K, or V as depicted on the Master Plan. The actual location of this street connection will be determined within 180 days of final project approval. A graphic representation of the actual location shall be provided to the Glen Oaks Neighborhood Association, the Wolf Pen Preservation Association, Inc., and a designated representative of Mint Springs Branch Road also within 180 days of final project approval.

DRAINAGE REPORT/STORMWATER MANAGEMENT

Existing Conditions

All of the Jefferson County portion of the Norton Commons project slopes and drains in a predominately uniform manner from east to west. There is no drainage flowing to the east under Old Brownsboro Road (Kentucky Highway 1694). Drainage exits this property at four locations:

1. Under Chamberlain Lane 800 feet north of I-71;
2. Under Chamberlain Lane 3,200 feet north of I-71;
3. Across a property line 200 feet north of the 90-degree turn on Chamberlain Lane; and
4. Across the Oldham County line 1,400 feet east of the western boundary of Norton Commons.

Stormwater Management Commitments

A Stormwater Management Concept Plan Map is included on the following page 21(a) as a graphic illustration of the commitments and system characteristics described below. This map is intended for illustrative purposes only and is not intended to be a final representation of the boundaries or location of stormwater facilities represented on the map. The final boundaries, location and physical characteristics of such facilities will be determined during the final design process to produce construction documents that will be utilized for permitting through the Metropolitan Sewer District ("MSD"), Oldham County Sanitation District or other affected governmental agency. However, these facilities will be designed consistent with the commitments described below.

The conceptual stormwater management plan has been developed to address several issues and requirements including: the requirement of MSD governing the rate of stormwater discharge to receiving bodies; natural and structural controls to minimize increases in the volume of discharge to receiving bodies; the control of "first flush" stormwater flow as a means of positively affecting the quality of water that is discharged to receiving bodies; the creation of on-site regulatory or non-regulatory wetlands as a means of treating the "first flush" of stormwater, providing habitat for wildlife and maintaining/improving groundwater/aquifer recharge; the preservation of on-site springs, regulatory and non-regulatory wetlands, and habitat; and the integration of the stormwater management system with planned park and open space resources and the internal transportation network.

First flush shall be defined as up to the initial first one-inch of rainfall that rinses pollutants off impervious surfaces, which shall be determined as follows:

- for parking lots, roads, driveways, and sidewalks, the first flush shall be the volume of runoff that includes at least 80% of the suspended sediment load on an average, annual basis. In all circumstances, the first flush volume of runoff shall not exceed one-inch.
- for rooftops, the first flush shall be the volume of runoff that is equal to the first one-half inch (0.5") of rainfall.

Rate of Stormwater Discharge

The final stormwater management plan shall ensure that post-development peak runoff rates of stormwater discharge leaving the development site shall not exceed peak pre-development rates (see PVD Ordinance, page 12, Sec. 1.1).

Natural and Structural Controls to Minimize the Volume of Stormwater Discharge

MSD does not currently have a specific requirement in the Harrod's Creek Watershed for stormwater volume control. However, volume control can be an important element in the maintenance and proper functioning of downstream receiving bodies. Norton Commons shall develop an off-line stormwater retention system that shall permanently capture and contain on-site the first flush of runoff from impervious surfaces.

Natural and structural methods will be evaluated and utilized, where practical, to transmit or retain this retention stormwater, such as creation of on-site regulatory and non-regulatory wetlands in association with, but separate from, detention basins; bioretention facilities located within road medians and rights-of-way; underground infiltration of first flush stormwater through a system of greenways; removal of curbs on roadways that abut elements of the parks and open space system that may contain regulatory or non-regulatory retention wetlands in order to promote overland flow of the runoff to increase infiltration and plant uptake of nutrients; and traditional storm sewer structural systems.

A combination of these potential methods is intended to increase the rate of upland recharge (infiltration) and evapotranspiration of stormwater - which is an indirect method of volume control. These features are also intended to permanently retain on-site a significant volume of stormwater from the first flush in the created wetlands associated with the outfall locations.

Reforestation, as illustrated on the stormwater management plan and a reforestation implementation schedule shall be submitted to the Citizens Advisory Committee within 180 days of final plan approval. The Citizens Advisory Committee shall have five working days to review the plan and provide comments to the Developer. Prior to or concurrent with the beginning of initial construction, the Developer shall implement the reforestation program in accordance with the final plan approved by the Citizens Advisory Committee and applicable regulatory agencies. The reforestation plan shall be based on replicating a native forest including species type, composition, and density, and shall contain agreed upon warranty and maintenance conditions that shall not exceed three years in length from the date of installation of each plant.

Provision and Conservation of Habitat and Integration with the Park and Open Space System

All proposed and existing regulatory or non-regulatory wetlands, detention ponds, special woodland study areas (see page 20(b) and bioretention facilities will become part of a planned, connected system of habitat areas that would support a wide variety of wildlife and vegetative species. In addition, existing on-site springs and seeps have been incorporated within the park and open space system and these locations will be protected. Native plant species will be utilized to the maximum extent feasible within the park and open space system as a means of fostering habitat for species that are indigenous to the Norton Commons area.

Stream Restoration

In the event that the stormwater management system does not function as planned or permitted, resulting in an adverse impact to a receiving stream, the Developer will be responsible for correcting the impact causing the adverse impact, and for correcting the damage caused by the adverse impact. The Developer will investigate any notification by the Citizens Advisory Committee representative of a potential adverse impact within forty-eight hours of such notification, and will provide a written plan of action to the Citizens Advisory Committee and appropriate permitting agencies within five working days of the initial investigation. Remedial action will begin within five (5) working days, weather permitting, of approval of the written plan of action by the Citizens Advisory Committee, affected property owners, and applicable permitting agencies.

Future Stormwater Planning

Norton Commons shall undertake a baseline water quality and bio-assessment of receiving streams in partnership with the MSD. This analysis will be performed within 180 days of final project approval and prior to any construction or site preparation, and the results furnished to the Planning Commission, MSD and Citizens Advisory Committee. Water quality and bio-assessment analyses shall be performed at least twice annually, typically in the late spring and late summer of each monitoring year, and results thereof shall be provided to the Planning Commission, MSD and the Citizens Advisory Committee.

These water quality and bio-assessment analyses shall serve as a benchmark for comparison with future analyses of water quality and bio-assessments in receiving streams following completion of the various phases of the project. An ongoing program of water quality and quantity monitoring will include water quality and bio-assessment evaluations, and ensure that a qualified construction inspection service or consultant shall be retained by developer to oversee construction and compliance with approved plans and binding elements.

A Stormwater Management Master Plan prepared by Developer's consultants will be submitted to the Citizens Advisory Committee prior to submittal to reviewing agencies. The Stormwater Management Master Plan shall include, at a minimum, provisions to address appropriate construction management and maintenance of the elements of the Stormwater Management Master Plan. The Stormwater Management Master Plan shall also ensure: that post development peak runoff rates of stormwater discharge leaving the development site shall not exceed pre-development rates; that the control of "first flush" stormwater flow shall be captured on site through such methods as regulatory and non-regulatory wetlands, detention basins, and bio-retention facilities within medians and rights-of-way as a means of positively affecting the quality

of water that is discharged to receiving bodies; the creation of on-site regulatory or non-regulatory wetlands as one of several means of treating the first flush of stormwater, providing habitat for wildlife and maintaining/improving groundwater/aquifer recharge; preservation of on-site springs, regulatory and non-regulatory wetlands and habitat; and, the integration of the stormwater management system with planned park and open space resources and the internal transportation network. *(Note: Please see Environmental Standards and Commitments on page 38 of this Master Plan Report.)*

Construction and Utility Easements

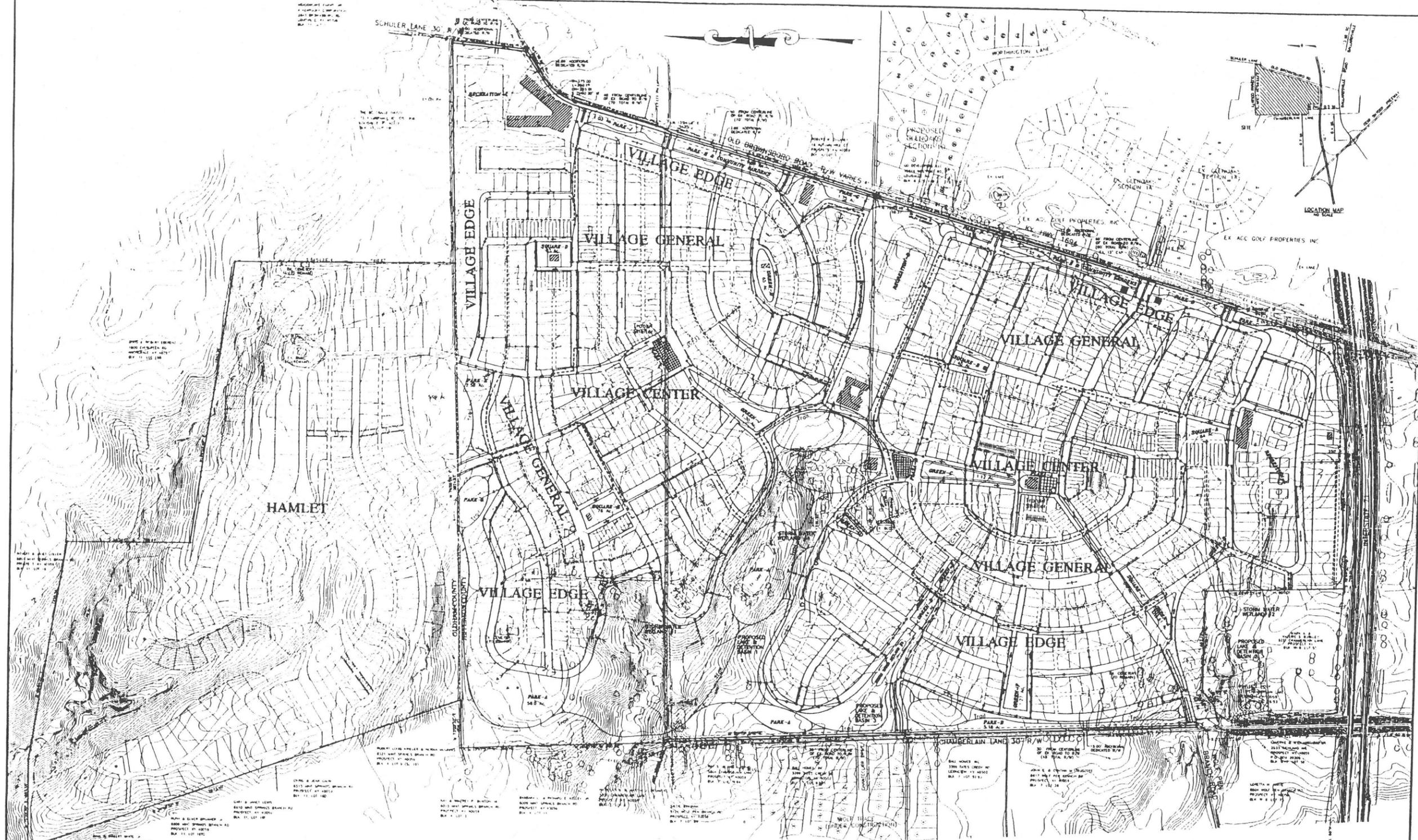
Applicable construction plans shall be presented to the Wolf Pen Preservation Association Inc., the Glen Oaks Neighborhood Association, and a designated representative of Mint Springs Branch Road indicating the location of utility easements prior to construction. The Developer shall restrict the use of heavy construction equipment in sensitive areas. Construction and utility easements and associated limits of construction shall be shown on the construction plans and shall be as narrow as practicable.

Wildlife and Vegetative Habitat Evaluation

The Developer shall perform a Wildlife and Vegetative Habitat Evaluation, and an evaluation of on-site areas for existing or potential wildlife habitat and shall be presented as a report to the Wolf Pen Preservation Association, the Glen Oaks Neighborhood Association, and a designated representative of Mint Springs Branch Road.

Landscape Plan for Frontage along Chamberlain Lane

The landscape plan for the frontage of the Norton Commons site for a distance of 100 feet from the Chamberlain Lane right-of-way shall be submitted to the Citizens Advisory Committee for review and comment at least 14 days prior to submission to the Division of Planning and Development Services ("DPDS"), or the Director of DPDS, or the Planning Commission or Committee thereof for approval.



- LEGEND**
- +2- EXISTING ELEVATION
 - - - - - EXISTING FENCE
 - - - - - EXISTING UTILITY POLE
 - - - - - EXISTING WATER
 - - - - - EXISTING GAS
 - - - - - EXISTING BURIED TELEPHONE
 - ▬▬▬▬▬▬▬ CMC BUILDINGS
 - ▬▬▬▬▬▬▬ BICYCLE/PEDESTRIAN TRAILS
 - ▬▬▬▬▬▬▬ BOUNDARY BETWEEN VILLAGE CENTER/GENERAL/EDGE
 - TRANSIT SHELTERS
 - - - - - PROPOSED STORM SEWER LINE & EASEMENT

STORM WATER NOTES

- DOWN STREAM CAPACITY ANALYSIS WILL BE REQUIRED PRIOR TO CONSTRUCTION APPROVAL. DOWN AND C/OE APPROVAL REQUIRED FOR DISTRIBUTION OF U.S. WATERWAYS. A REDUCTION IN STORM WATER RUNOFF WILL BE REQUIRED TO REDUCE THE EFFECTS OF INCREASED RUNOFF VOLUMES. EXISTING CONDITIONS SHALL INCLUDE EXISTING LADES. THERE SHALL BE NO INCREASE IN RUNOFF VELOCITIES TO PROPERTY LINE PLUMES OR DISCHARGE.
- FOR THE STORM DRAINAGE THAT ENTERS OLDSHAM COUNTY AT A POINT 1300 FEET EAST OF THE WESTERN BOUNDARY OF NORTON COMMONS, THE STORM WATER CONTROLS WILL BE LOCATED IN OLDSHAM COUNTY AND WILL BE DESIGNED IN ACCORDANCE WITH OLDSHAM COUNTY REQUIREMENTS. IF THIS IS IN ANY WAY UNACCEPTABLE TO OLDSHAM COUNTY OFFICIALS, THE STORM WATER CONTROLS WILL BE LOCATED IN JEFFERSON COUNTY.

STORM SEWER MANAGEMENT
 CONCEPT PLAN MAP
 FOR A
 PLANNED VILLAGE DEVELOPMENT DISTRICT
NORTON COMMONS
 DOCKET # 9-71-99

OWNER
 FNC BANK, KENTUCKY, INC.
 TRUSTEE UNDER THE WILL OF
 GEORGE W. NORTON, JR.

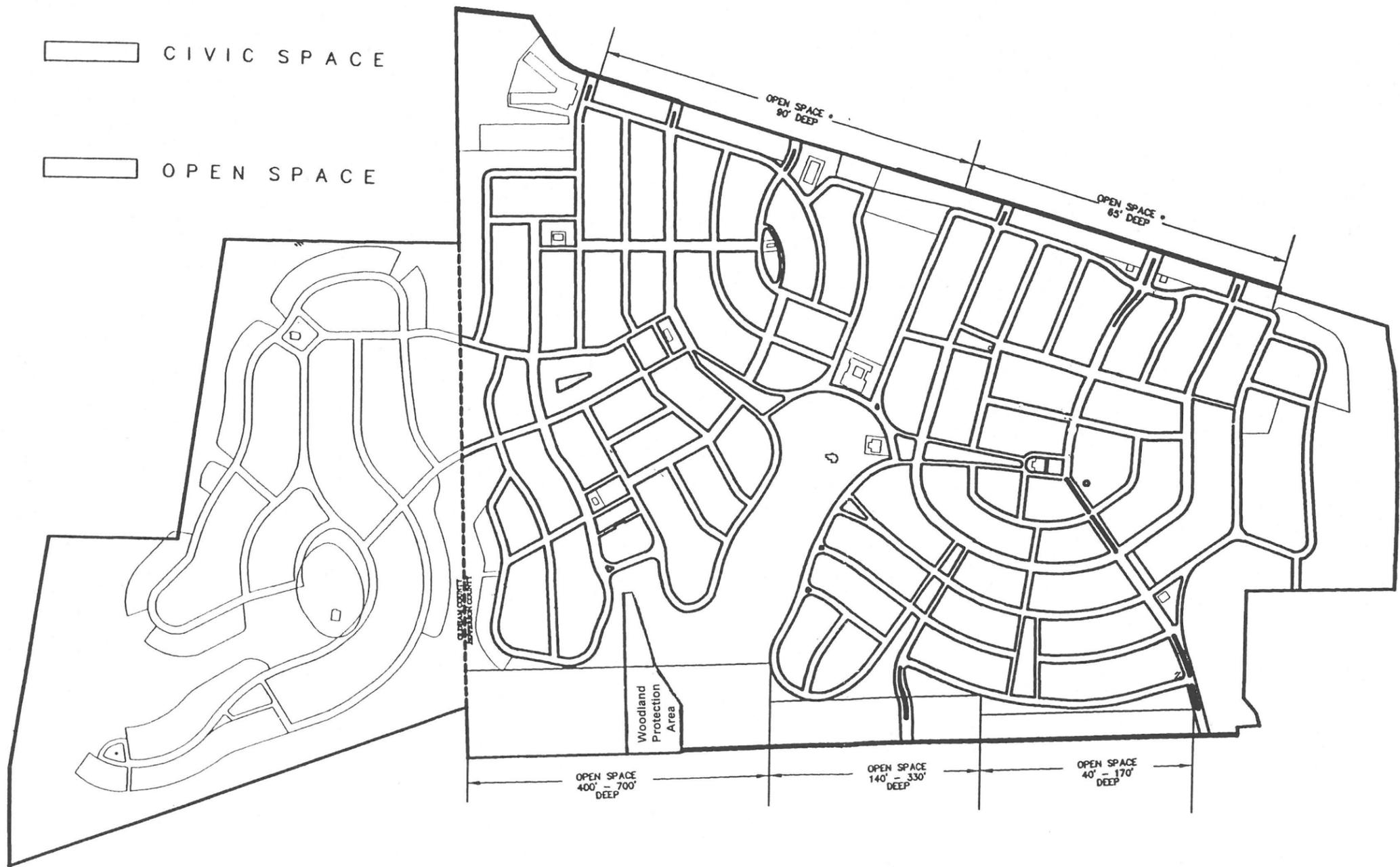
DEVELOPER
 NORTON COMMONS LLC
 1230 LIBERTY BANK LANE
 SUITE 230
 LOUISVILLE, KY. 40222

DUANY PLATER ZYBERK & COMPANY
 ARCHITECTS AND TOWN PLANNERS
 1000 BOUTWORTH SPUR AVENUE, SUITE 1000, MIAMI, FLORIDA 33130 (305) 666-1000

SABAK, WILSON & LINGO, INC.
 ENGINEERS, LANDSCAPE ARCHITECTS & PLANNERS
 915 WEST MARKET STREET, LOUISVILLE, KENTUCKY 40202 (502) 581-4271

▭ CIVIC SPACE

▭ OPEN SPACE



• EXCLUDES CMC USES



SANITARY SEWAGE FACILITY REPORT

All sanitary sewage from Norton Commons will be conveyed to the MSD Hite Creek Treatment Plant.

The main pump station, station A, will be located in the vicinity of the existing 90-degree bend in Chamberlain Lane. The force main from this station will run east through the open area between the two villages, then south along the west side of Old Brownsboro Road (Kentucky Highway 1694) to the north side of I-71; then east along the north side of I-71 in an existing sewer easement, to the vicinity of Hite Creek; then through a proposed bore under I-71 to the Hite Creek Treatment Plant.

The first section of Norton Commons will be served by pump station B located in the vicinity of the proposed entrance to Norton Commons from Chamberlain Lane. Glen Oaks subdivision is served by one main pump station that is connected to the Hite Creek Treatment Plant. Sabak, Wilson & Lingo, Inc. has made previous studies and inquiries that reveal that the existing gravity sewer near the intersection of Stone School Road and Old Brownsboro Road in Glen Oaks has the capacity to receive the flow from approximately 143 lots in Norton Commons. Therefore, Norton Commons proposes to connect pump station B to the existing Glen Oaks system, subject to MSD approval. If the flow from lift station B ever exceeds the capacity of the Glen Oaks system, then the flow from this pump station B can very easily be diverted to the gravity system in Norton Commons that is tributary to the main pump station A.

Pump stations A and B are the only ones needed for the Jefferson County portion of Norton Commons.

The Oldham County portion of Norton Commons will require three small lift stations. These stations will be connected to the gravity system tributary to main pump station A. These three stations are required due to the slopes and dense woods in the Oldham County valleys.

The *Phasing Map* on the following page shows 16 tentative sections based on a best estimate of future lot sales. Changing market conditions and buyer preferences will influence the actual areas of the site to be developed.

Open space will be recorded with each section to maintain a 26% ratio of open space to developed land areas. Open space improvements will be a valuable sales tool, and will be discussed with each phase's Final Plan to provide future residents appropriate recreational opportunities. Improvements will be made in the beginning and become more comprehensive as Norton Commons grows and there is more need for park features. Initially, open space will mainly be passive green spaces with some trails.

All utilities will, of course, be installed to the extent needed to serve each section. Storm water detention basin number 2 will be installed with the first section that discharges to basin number 2. Erosion prevention and sediment control measures and facilities will be installed for each section.

A small store/sales office will be constructed in square "A" during the first phase, with the first Village Center starting in the third phase.

Land Use Traffic Phasing

Development of the Norton Commons project will occur in phases, generally over a twenty year time period. Two types of project phasing are referred to in documents describing the project, including this *Master Plan Report for Norton Commons* and the *Traffic and Air Quality Impact Study*.

The *Master Plan Report* includes a Phasing Map (following page) that identifies sixteen (16) phases for the Jefferson County portion of the project. This graphic is included to satisfy a requirement of the Planned Village Development ordinance to identify the timing and location of development and associated infrastructure. The graphic is consistent with the intent of the master plan for development of two connected villages within the Jefferson County portion of the project, and a "hamlet" of lower density residential uses within the Oldham County portion. The first eight land use/infrastructure phases address the area of the site proposed for the first village, and the second eight comprise village two.

Since these "phases" are intended to generally address how the land use components of a village (center, general and edge) and project infrastructure will be developed, there are no specific descriptions of the number of dwelling units or square footage of non-residential uses within each phase. This type of information will be available with final site/subdivision plans.

The Norton Commons *Traffic and Air Quality Impact Study* also addresses project phasing, but for a different purpose. The methodology for determining project related transportation and air quality requirements is based on estimates of the amount of development that is expected to occur within given periods of time. Such estimates were developed for Norton Commons and are identified below. The phases are cumulative.

Traffic and Air Quality Phase 1 – Year 2005

- | | |
|---------------------------------|---------------------------|
| 1) Freestanding Commercial Uses | 70,000 gross square feet |
| 2) Office and Civic Uses | 125,000 gross square feet |
| 3) Single and Multi-Family Uses | 880 dwelling units |

Traffic and Air Quality Phase 2 – Year 2010

- | | |
|---------------------------------|---------------------------|
| 1) Freestanding Commercial Uses | 155,000 gross square feet |
| 2) Office and Civic Uses | 260,000 gross square feet |
| 3) Single and Multi-Family Uses | 1,850 dwelling units |

Traffic and Air Quality Phase 3 – Year 2020

- | | |
|---------------------------------|---------------------------|
| 1) Freestanding Commercial Uses | 200,000 gross square feet |
| 2) Office and Civic Uses | 360,000 gross square feet |
| 3) Single and Multi-Family Uses | 2,880 dwelling units |

There is a general correlation between *Infrastructure Phasing* and *Land Use and Traffic Phasing* to the extent that the location and timing of land use development will be guided by infrastructure investments. However, the phasing information should not be construed as exact. It may take more or less time to complete the uses anticipated for any Traffic and Air Quality Phase, and the location of uses may not follow exactly the sequence of infrastructure phases as depicted on the *Phasing Map*. Market conditions and other factors will ultimately affect the physical development of the project.

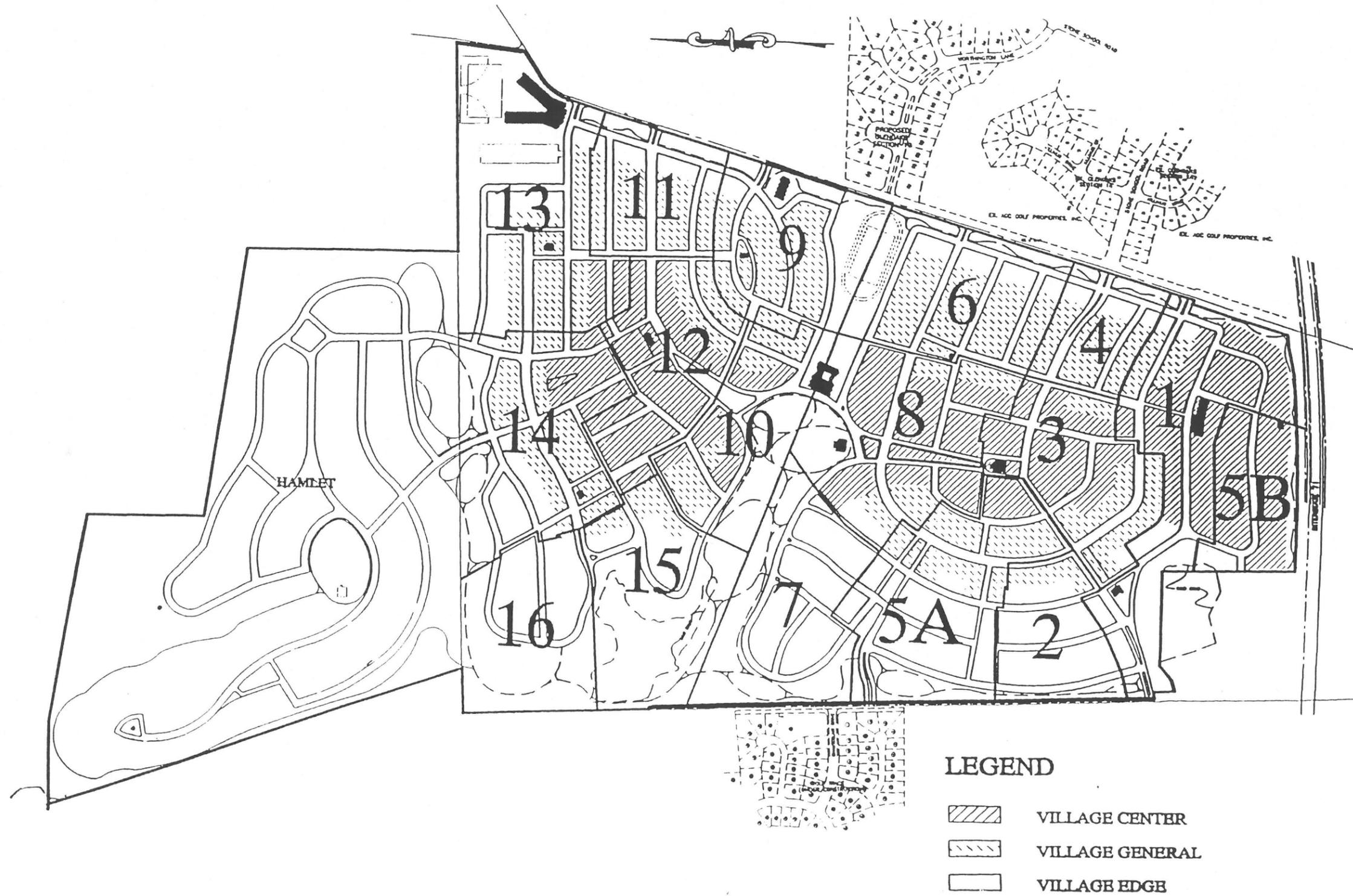


TABLE OF PERMITTED, LIMITED AND PROHIBITED USES

The following table lists the uses permitted within a village. **P** means the use is permitted, subject to design standards and location standards where noted. **L** means the use is limited. Limited uses must be approved in accordance with the procedures that follow this Table. **X** means the use is not permitted. No permitted use or limited use shall be required to secure a conditional use permit.

Residential Use	Village Edge	Village General	Village Center	Civic Use	Village Edge	Village General	Village Center
Detached dwelling	P	P	P	Churches, parish halls and temples	L	L	L
Semi-detached dwelling	X	P	P	Clubs, private proprietary	L	L	L
Attached dwelling	X	P	P	Colleges, schools and institutions of learning (except trade, business or industrial schools), not for profit	L	L	L
Two-family dwelling	X	P	P	Community residence	L	L	L
Multi-family dwelling	X	P ¹	P	Convents and monasteries	L	L	L
Accessory resid. unit	P	P	P	Day care center	L	P ¹	P
Live/Work unit	X	L ²	P	Family day care center	P	P	P
Lodging Use				Family care home	P	P	P
Bed and breakfast inn	L	L	P	Nursing homes and homes for the infirm and aged	L	L	L
Hotel	X	X	P	Historical buildings and grounds	P	P	P
Office Use				Libraries, museums, arboretums and art galleries, not for profit	L	L	L
Home occupation	P	P	P	Meeting hall	L	L	L
Office	X	P ³	P	Other			
Commercial Use				Agriculture	P	P	P
Neighborhood Commercial/service ⁴	X	P ³	P	Garage sale	P	P	P
General commercial ⁵	X	X	P	Outdoor advertising sign	X	X	X
				Utilities	L	L	L

¹Only at intersections where at least one street is designated as the highest classification of streets in the village.

²Only along major streets. Major streets are defined as all of the highest street classifications (AV-1, AV-2, AV-3, AV-4 and S-1).

³Only at intersections where at least one street is included in the Highest Classification of Streets (AV-1, AV-2, AV-3 and AV-4) as shown on the Master Plan, and only on the first floor. Office or other commercial use shall not exceed 50% of the floor area of the entire structure. The remaining floor area shall be residential.

⁴Includes only permitted and special permitted uses in the CR zone, not subject to CR requirements. Drive-through facilities, commercial towers (including cellular towers), on-site dry cleaning plants, and automobile service stations are not permitted. Single retail uses with a building footprint that is over 50,000 square feet shall be discouraged.

⁵Includes only uses permitted in the C1 zone plus neighborhood pubs and live music in restaurants. Drive-through facilities, commercial towers (including cellular towers), on-site dry cleaning plants, automobile service stations, and car washes are not permitted. Single retail uses with a building footprint that is over 50,000 square feet shall be discouraged.

Limited Uses

Section 2.1.5 of the Planned Village Development Ordinance ("PVD Regulation") of the Louisville and Jefferson County Development Code, indicates that the Planning Commission must **approve limited uses** at the time of approval of the Master Plan. Subsequent requests for limited uses shall require amendment of the Master Plan and shall be reviewed according to Section 3.2 of the PVD Regulation. The following procedures and criteria have been incorporated in this Master Plan Report and shall be applicable to the project.

- 1) Approved *limited uses* for the Norton Commons development shall be identified either by reference on the face of the Master Plan (as originally approved or amended through action of the Planning Commission pursuant to the provisions of Section 3.2 of the Planned Village Ordinance), or by application of the criteria identified below.
 - a) All types of *limited uses* may be permitted within a Village Center subject to the following conditions and/or limitations:
 1. *Limited uses* may not occupy more than twenty (20) percent of the net land area (gross land area less road rights-of-way and designated open space) of a Village Center.
 2. The location of *limited uses* within the Village Center is not restricted, except for land parcels that adjoin the edge of the Center as depicted on the Master Plan. For these parcels, *limited uses* shall be permitted on parcels that meet any of the criteria specified in (b).
 - b) All types of *limited uses* may be permitted within the Village General. Limited uses shall be restricted to those locations depicted on the face of the Master Plan, or to any site that meets one of the following criteria: the site location will deflect a vista (see Illustration for a deflected vista); the site location will terminate a vista (see Illustration for a terminated vista); or the site is located at a street intersection where at least one street is included in the Highest Classification of Streets (AV-1, AV-2, AV-3, AV-4) as shown on the Master Plan.

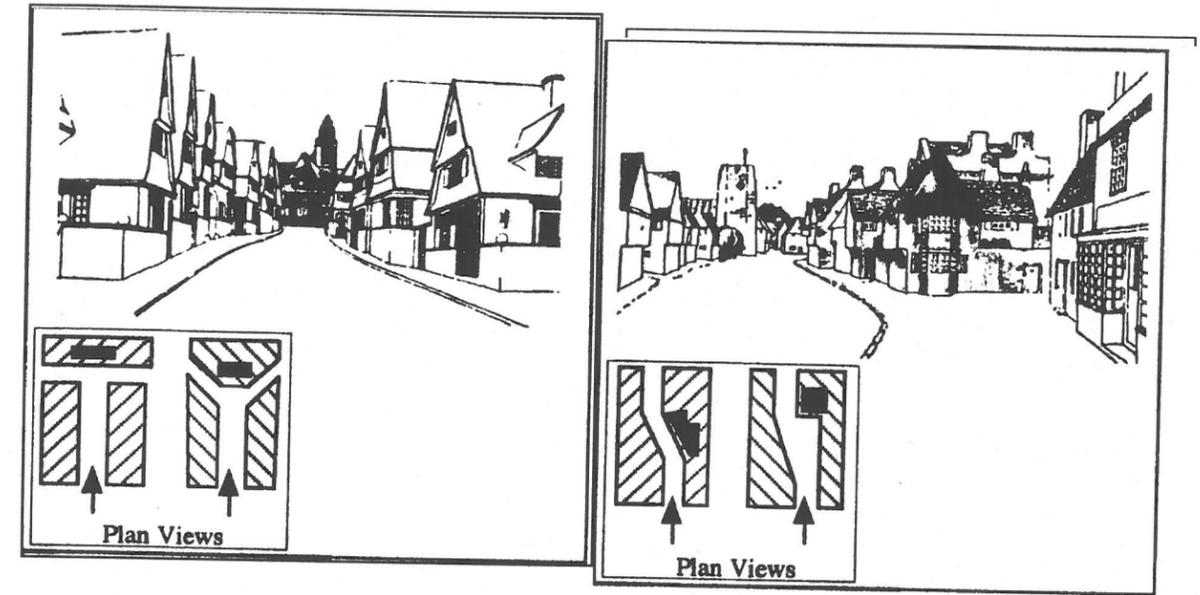


Illustration of a Terminated Vista

Illustration of a Deflected Vista

- c) All types of *limited uses* may be permitted within the Village Edge subject to the following conditions and/or limitations:
 1. *Limited uses* shall be restricted to those locations depicted on the face of the Master Plan, or to any site that abuts a designated open space area or civic use.
 2. *Limited use* sites, whether depicted on the Master Plan or consistent with the criteria contained herein, shall be depicted on the Final Plan for the applicable portion of the development. The physical boundaries of each site and the designated *limited use(s)* shall be identified on the Final Plan.
 3. Subsequent to approval of a Final Plan, no additional *limited use* sites may be designated within the area of the approved Final Plan except as provided by Section 3.2 of the Planned Village Ordinance (amendment of Master Plan).
 4. The boundaries of *limited use* sites identified and approved as part of a Final Plan may be modified through a Revised Final Plan that is reviewed and approved by the Technical Review Committee. Modifications that may be approved through this process shall be limited to fifteen (15) percent increase in the limited use site area.
 5. No building permit for a limited use approved by the Planning Commission shall be issued without the prior review and approval of the Norton Commons Village Architect.

STANDARDS

< R U R A L | | | | | | | | | | | | | | | | | U R B A N >

	VILLAGE EDGE	VILLAGE GENERAL	VILLAGE CENTER	ALL VILLAGE COMPONENTS	PROHIBITED USE IN ALL VILLAGE COMPONENTS
			OPEN USE	CIVIC USE	PROHIBITED USE
<p>RESIDENTIAL Premises available for long-term human habitation by means of ownership and rental, but excluding short-term letting of less than a month's duration.</p>	<p>Residential: The number of dwellings is restricted to one within the principal building and one within an accessory residential unit, and by the requirement of two assigned parking spaces for the principal dwelling and one for the accessory residential unit. Both dwellings shall be under single ownership, sharing a single set of utility connection. The habitable area of the accessory dwelling shall not exceed 600 sq. ft., excluding outdoor deck space, porches, outdoor storage space and outdoor stairwells. Habitable area means the area of the unit as measured from the interior of the exterior walls and excluding interior wall space.</p>	<p>Residential: The number of dwellings is restricted to one within the principal building and one within an accessory residential unit, and by the requirement of two assigned parking spaces for the principal dwelling and one parking space for the accessory residential unit. Both dwellings shall be under single ownership, sharing a single set of utility connections. The habitable area of the accessory dwelling shall not exceed 600 sq. ft., excluding outdoor deck space, porches, outdoor storage space and outdoor stairwells. Habitable area means the area of the unit as measured from the interior of the exterior walls and excluding interior wall space.</p>	<p>Open Residential: The number of dwellings is limited by the requirement of two assigned parking spaces for each dwelling, a ratio which may be reduced according to the shared parking standard (e.g., Apartment Buildings).</p>	<p>Religion Arts and Culture Education Government Social Service Recycling Transit Shop Public and Private Health Fitness Centers Recreation (such as amphitheater and community center)</p>	<p>Drive Through Commercial where patrons remain in automobiles. Vending machines, except within buildings. Freestanding signs (except for real estate rent/sale signs and development identification signs, which are permitted) and billboards. Regional Retail - including "Big-Box" retailing with front parking lot offering wide choice, often at reduced prices. Industrial - emanating noise, vibration or smell beyond the boundary of its site. Kennels and animal husbandry. Prisons, except as accessories to police stations. Terminals for large scale transportation including airports. Depots for large scale storage of distribution of goods. Scrap Yards for the processing, storage and disposal of waste materials, excepting recycling collection centers. Automotive - including sales, repair, and service stations. Car washes. Golf Courses. Mineral Extraction or mining. Commercial Towers (including cellular towers). On-site dry cleaning plants.</p>
<p>LODGING Premises available for short-term human habitation, including daily and weekly letting.</p>	<p>Lodging: The number of bedrooms available for lodging is restricted to one within an accessory residential unit, and by the requirement of one assigned parking space for each leaseable bedroom in addition to the parking requirement of two spaces for the dwelling (e.g., Guest Cottage).</p>	<p>Lodging: The number of bedrooms available for lodging is limited by the requirement of one assigned parking space for each bedroom, in addition to the parking requirement for each dwelling. Food service may be provided only before 11 a.m. (e.g., Bed & Breakfast Inn).</p>	<p>Open Lodging: The number of bedrooms available for lodging is limited by the requirements of one assigned parking space for each bedroom, a ratio which may be reduced according to the shared parking standards. Food service may be provided at all times (e.g., Boarding House or Hotel).</p>		
<p>OFFICE Premises available for the transaction of general business, including professional and information services, but excluding retail sales and manufacturing activity.</p>	<p>Office: The area available for office use is restricted to the accessory residential unit, and by the requirement of one assigned parking space for each 250 sq. ft, in addition to the parking requirement for each dwelling (e.g., Home Occupation).</p>	<p>Office: The area available for office use is limited to the first story of the principal building and/or to the accessory residential unit, and by the requirements of one assigned parking space for each 250 sq. ft, in addition to the parking requirements for each dwelling (e.g., Home Office).</p>	<p>Open Office: The area available for office use is limited by the requirement of one assigned parking space for each 250 sq. ft of gross office space, a ratio which may be reduced according to the Shared Parking Standards (e.g., Corporate Office).</p>		
<p>RETAIL Premises available for the commercial sale of merchandise and prepared foods and other uses permitted by the PVD District and the Master Plan Report.</p>	<p>Retail: Retail use is not permitted.</p>	<p>Limited Retail: The area available for retail use is limited to the first story of buildings at corner locations. The specific use shall be further limited to Neighborhood Store, Day Care or Food Services seating no more than 40 patrons at one time.</p>	<p>Open Retail: The area available for retail use is limited to the first story of buildings and by the requirement of one assigned parking space for each 250 sq. ft of gross retail space, a ratio which may be reduced according to the Shared Parking Standards (e.g., Shopfront, Store).</p>		

VILLAGE EDGE			VILLAGE CENTER					
Villa	House	Cottage	VILLAGE GENERAL					Commercial Building
			Single House	Patio House	Apartment Building	Rowhouse	Flexhouse	Commercial Building
<p>Villa An Edge Yard residential building type. A single-family dwelling on a very large separate lot of rural character, often supplemented with an Ancillary Building. <i>Syn.</i> Estate House</p>	<p>House An Edge Yard residential building type. A single-family dwelling on a separate lot, possibly shared with an Ancillary Building to the rear yard.</p>	<p>Cottage An Edge Yard residential building type. A single-family dwelling, smaller than a House, on a separate lot.</p>	<p>Single House A Side Yard residential building type. A single-family dwelling which occupies one side of the lot, with the primary yard to the other side. The side yard on the street frontage causes this building type to appear freestanding. If the adjacent building is also a Side Yard type with a windowless common wall, the open space can be quite private. This type permits systematic climatic orientation with the long side yard elevation responding to the sun or the breeze. <i>Variant:</i> Double house</p>	<p>Patio House A Court Yard residential building type which surrounds one or more private yards. This is a most resilient type as it is able to shield the private open space from a public realm of great intensity. This type is common in hot climates, but its general attributes are useful everywhere. Because of its ability to internalize the activities of its yard, it is recommended for workshops, lodging, and schools. The security provided by the surrounding of the yard is useful for crime-prone sectors. <i>Syn.</i> Courtyard House</p>	<p>Apartment Building A Rear Yard residential building type accommodating multiple dwellings disposed above and beside each other, sharing a common entry. Apartment Buildings show typological variations: arranged as Perimeter Blocks, Urban Villas and Courtyard Apartments, the latter two are usually compatible with Houses. An Apartment Building may provide private open space to most dwellings with balconies. <i>Variant:</i> Loft Building</p>	<p>Rowhouse A Rear Yard residential building type. A single-family dwelling with common walls on the side lot lines, the facades reading as a continuous streetwall. Rowhouses are the highest density type able to provide private yards and fee-simple ownership. A set of Rowhouses creates yards with adequate privacy only when equipped with back-buildings. Usually served by alleys. <i>Syn.</i> Townhouse, Terrace House</p>	<p>Flexhouse A Rear Yard, Flexible Commercial building type with one dwelling attached to a commercial loft. Common walls are permitted on the side lot lines. Flexhouses may be located at Mandatory Retail Frontages. They are usually confined to Center Zones. Always served by alleys. <i>Syn.</i> Store, Live-Work Unit, Corner Store, Shop-front.</p>	<p>Commercial Building A Rear Yard, Fixed Commercial building type with commercial use throughout. Common walls are permitted on the side lot lines. Commercial Buildings are among the largest urban types as they must accommodate the floorplates of the modern workplace. They are usually confined to Center Zones. Always served by alleys. <i>Syn.</i> Warehouse, Flexspace, Office Building.</p>

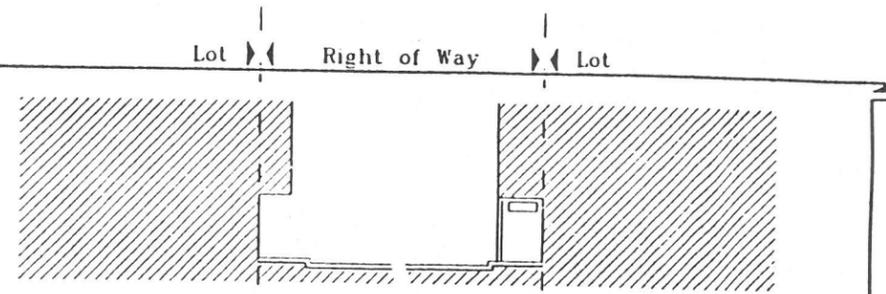
URBAN SUBURBAN RURAL

PRIVATE FRONTAGES

Arcade

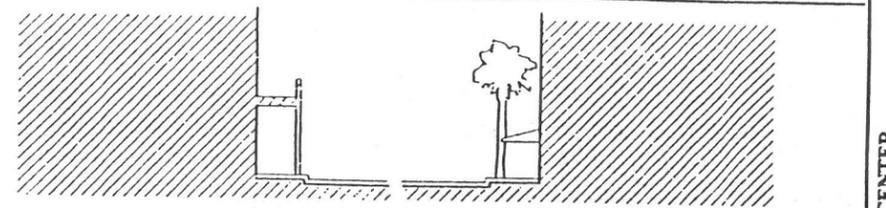
The building overlaps the sidewalk above with upper stories while the ground story remains set back at the lot line. This type is indicated for retail use, but only when the sidewalk is fully absorbed within the arcade so that a pedestrian cannot

bypass it. An easement for private use of the right-of-way is usually required. To be useful, the arcade should be no less than 12 ft. wide.



Shop

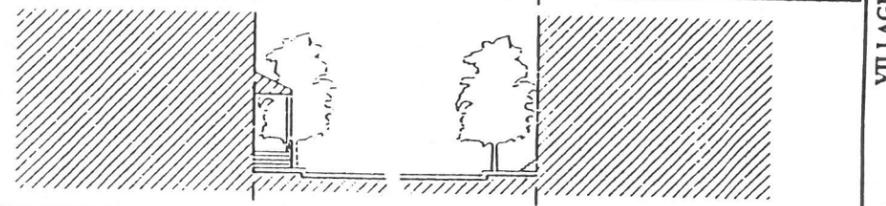
The facade is aligned close to the frontage line with the entrance at sidewalk grade. This type is conventional for retail frontage. It is commonly equipped with cantilevered shed roof, an awning or an attached colonnade.



Stoop

The facade is aligned close to the frontage line with the ground story elevated from the sidewalk securing privacy for the windows. This type is suitable for ground floor residential uses at short setbacks with rowhouses and apartment build-

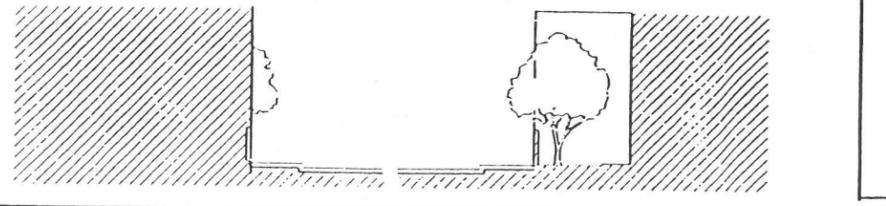
ings. An easement may be necessary to accommodate the encroaching stoop. This type may be interspersed with the Shopfront.



Forecourt

The facade is aligned close to the frontage line and a portion if it is set back. The forecourt thus created is suitable for gardens, vehicular drop-offs, and utility off-loading. This type should be used sparingly and in conjunction with the two

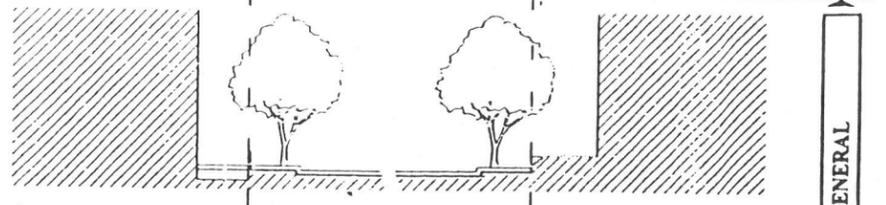
frontage types above, as a continuous excessive setback is boring and unsafe for pedestrians. Trees within the forecourts should be placed to have their canopies overhanging the sidewalks.



Dooryard

The facade is set back from the frontage line with an elevated garden or terrace or, a sunken light court, in between. This type can effectively buffer residential quarters from the sidewalk, while removing the private yard from public

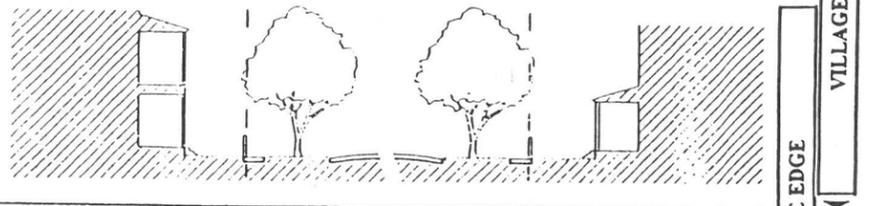
encroachment. The terrace is suitable for restaurants and cafes as the eye level of the sitter is level with that of the passerby standing. The light over can give light and access to a basement.



Porch and Fence

The facade is set back from the frontage line with an encroaching porch appended. The porch should be within a conversational distance of the sidewalk while a fence at the frontage line maintains the demarcation of the yard. To be

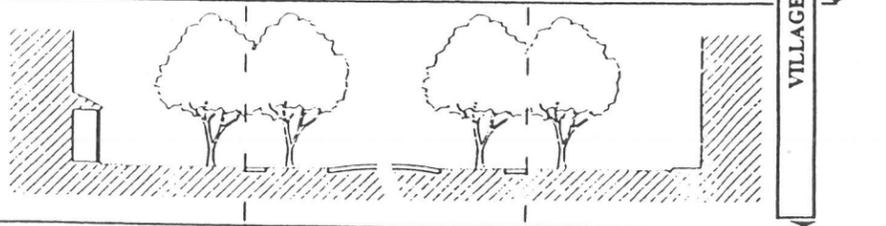
useful, the porch should be no less than 8 ft. wide. There is a great variety of porches.



Common Lawn

The facade is set back substantially from the frontage line. The front yard thus created should remain unfenced and be visually continuous with adjacent yards. The ideal is to simulate buildings sitting in a common rural landscape. A front porch is not required, as social interaction from the

enfronting thoroughfare is unlikely at such a distance. Common Lawns are suitable frontages for higher speed thoroughfares, as the large setback provides a buffer from the traffic.

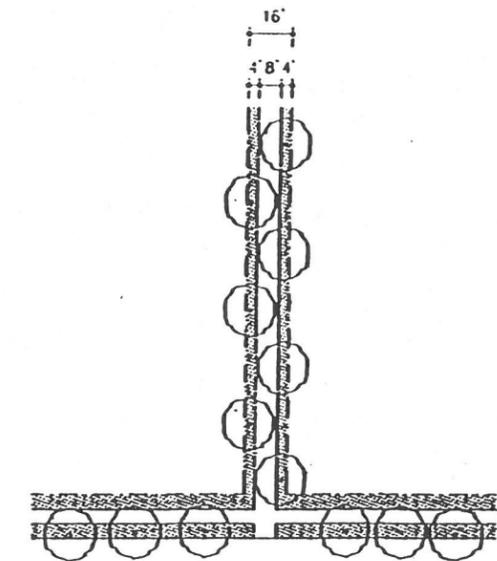
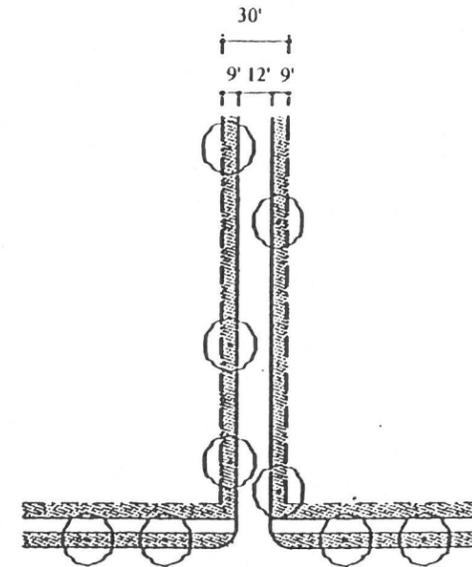
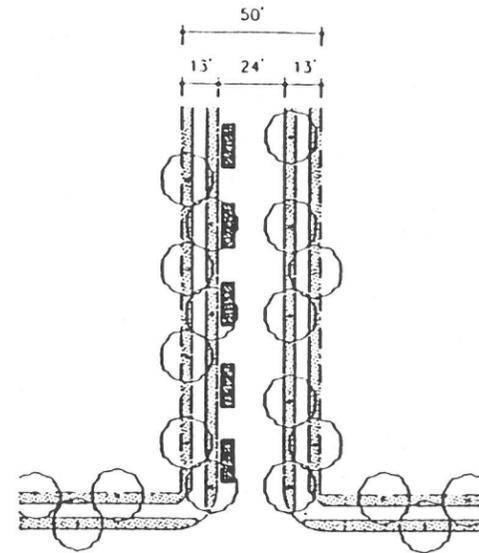
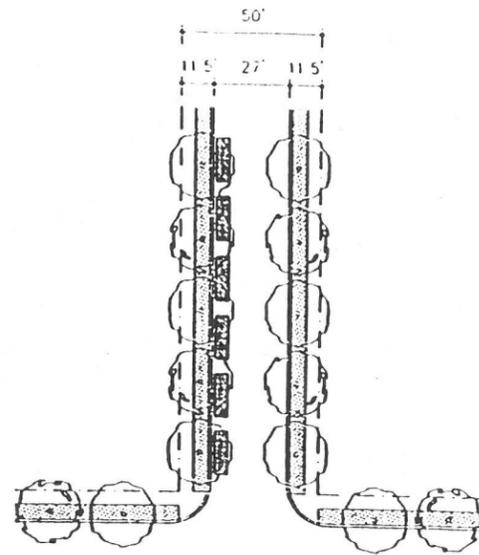


Lot Right of Way Lot

VILLAGE CENTER

VILLAGE GENERAL

VILLAGE EDGE



S-1

Large Residential Street
Free movement
Two way traffic
Parking one side
Closed section
50 ft. ROW
27 ft. pavement width
15 ft. curb radius
30 MPH
<5,000
6.7 seconds pedestrian crossing time
5ft. sidewalks
6.5 ft. continuous planters
Allee 30 ft. o.c./ 34 ft. across

R-1

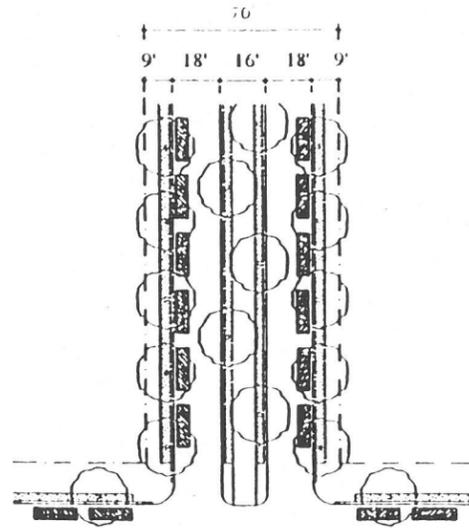
Large Residential Road
Slow movement
Two way traffic
Parking one side
Swale section
50 ft. ROW
24 ft. pavement width
10 ft. curb radius
25 MPH
<5,000 vehicular capacity
7.4 seconds pedestrian crossing time
5 ft. sidewalks
4 ft. continuous planters
Clusters of 3 - 5 trees

A-1

Alley
Yield movement
Two way traffic
No Parking
Open section
30 ft ROW
12 ft. pavement width
8 ft. curb radius
15 MPH
<1500 vehicular capacity
2.7 seconds pedestrian crossing time
No sidewalk
6 ft. continuous planter
Interstitial planting

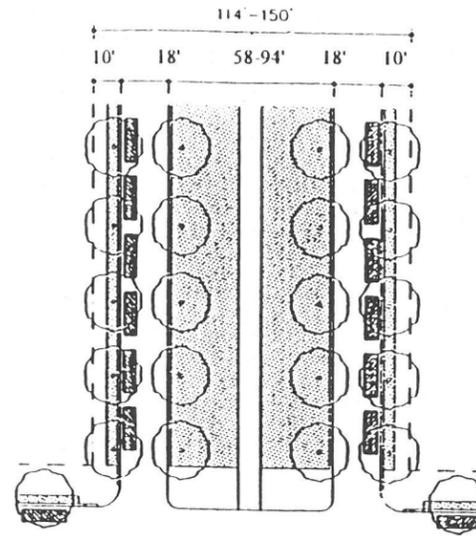
C-1

Passage
Pedestrian movement
No traffic
No Parking
Open section
16 ft. ROW
NA
4-8 ft sidewalk
4 ft. continuous planters
Staggered allee planting pattern



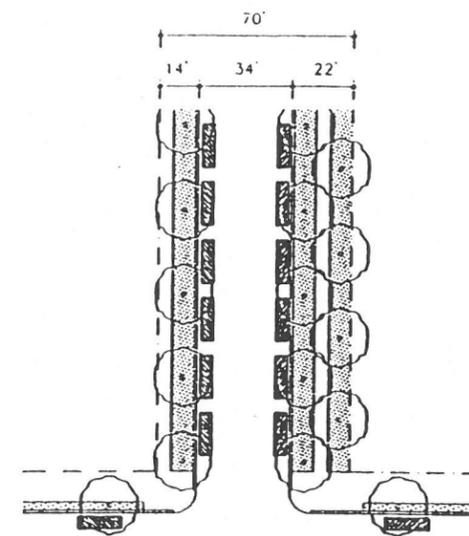
AV-1

Avenue with Bike Trail
Speed movement
Two way traffic
Parking both sides
Closed section
70 feet ROW
18 ft. & 18 ft. pavement widths
10 ft. curb radius
35 MPH
<12,000 vehicular capacity
11.4 seconds pedestrian crossing time
5 ft. sidewalks & 8 ft. bike path
4 ft. continuous planters & 16 ft. median
Staggered allee 27 ft. o.c./20 ft. across



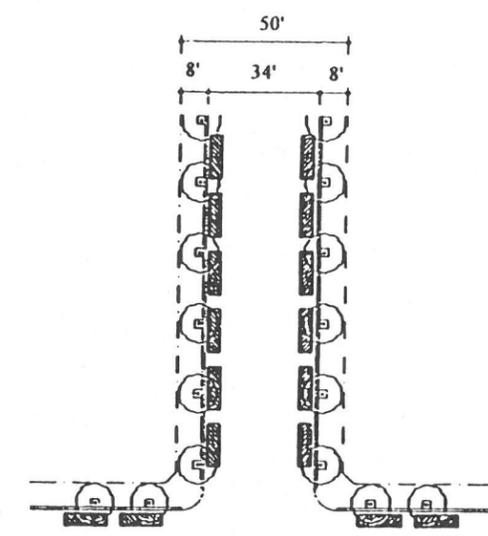
AV-2

Avenue with Bike Trail
Speed movement
Two way traffic
Parking both sides
Closed section
114 ft. - 150 ft. ROW
18 ft. & 18 ft. pavement width
10 ft. curb radius
35 MPH
<12,000 vehicular capacity
11.4 seconds pedestrian crossing time
5 ft. sidewalks & 8 ft. bike path
4 ft. continuous planters & 60 ft.- 96 ft. median
Allee 25 ft. o.c./25 ft. across



AV-3

Avenue with Jogging Trail
Speed movement
Two way traffic
Parking both sides
Closed section
70 ft. ROW
34 ft. pavement width
10 ft. curb radius
35 MPH
<12,000 vehicular capacity
11.4 seconds pedestrian crossing time
5 ft. sidewalks
9 ft. continuous planter
Allee 30 ft. o.c./40 ft. across



AV-4

Large Commercial Street
Free movement
Two way traffic
Parking both sides
Closed section
50 ft. ROW
34 ft. pavement width
8 ft. curb radius
30 MPH
<12,000 vehicular capacity
8.1 seconds pedestrian crossing time
8 ft. sidewalk
4 ft. x 4 ft. individual planters
Allee 25 ft. o.c./45 ft. across

INSTRUCTIONS FOR THE PUBLIC LANDSCAPE

Soil Preservation. Grades for thoroughfare and open spaces shall generally follow existing topography and drainage patterns. Public places shall remain fenced and undisturbed during construction. The deep soil structure of wide planting strips shall be protected from compaction with stakes and by establishing standards of access and soil movement for deep utilities and manholes. The topsoil of construction areas shall be removed, stored and amended with organic matter, as needed, for later use.

Cover: Squares shall be carefully graded, leveled and planted with a dwarf bluegrass species mix. Playing fields and high use areas shall include appropriate fescue varieties in the mix. Greens and Rural Greens shall be planted or managed with appropriate low care and drought tolerant turf grasses cut high. Fertilization shall be yearly, in spring, with a full spectrum balanced tree fertilizer with 100% water insoluble organic nitrogen. The cover in park shall be meadow types which shall remain unfertilized except for the initial seeding stage.

Nursery. Stated cultivars shall be searched by a plant broker before consideration of alternative cultivars. Public trees in the Village Center and Village Edge shall range from 10 to 16 feet high, lower branches pruned one month before planting. Scarce cultivars and native trees are exempted from the height requirement. Extra trees shall be planted at an on-site field nursery for replacement.

Planting Procedures. All transplanted trees shall be sprayed with anti-transpirant before movement in late winter. No planting hold amendment other than the area amendment of de-compaction procedures shall be permitted. Otherwise follow the highest industry standards.

INSTRUCTIONS FOR THE PRIVATE LANDSCAPE

Soil Preservation. Existing topsoil from the building footprint shall be preserved. The remaining soil profile shall be protected from deep compaction during building construction by mandating and staking alley or lane access during construction. De-compaction and hydrological permeability of compacted soil areas shall be achieved by mechanically breaking up remnant basement soil and rototilling 2-3 inches of recycled organic matter, before the addition of a mix of organically amended topsoil.

Planting Code. One species or cultivar of tree from the following lists shall be planted for every 30-35 feet of frontage or fraction. Planting other tree species is permitted, but shall not count toward the fulfillment of the code requirements or the objective of establishing a visually coherent long-term spatial structure for microclimate and wildlife that is supportive of the public landscape.

Size. Acceptable tree heights on planting shall vary according to species and availability and shall be determined by the Town Architect.

Placement. Frontage trees shall be placed within 10 feet of the lot frontage line and its extension. Alley trees shall be placed four feet on either side of the back lot line. Yard trees in the Village Edge can be placed anywhere in the property, except one species must be placed within eight feet of the back lot line, on either side, to constitute the lane.

Substitution. One required tree may be substituted by a hedge along the side of the property lines.

VILLAGE EDGE COMPONENT

PUBLIC TRACTS

Parks

Bur Oak -*Quercus macrocarpa*
Swamp White Oak -*Quercus bicolor*

Drive (AV-4)

Resistant American Elm-*Ulmus americana* var. "Delaware II," "Iowa State," "Liberty," "Washington";
Tuliptree - *Liriodendron tulipifera*

Large Road (S-1)

Cherrybark Oak-*Quercus pagodaefolia*
"Monarch of IL" Bald-Cypress *Taxodium distichum* var. "Red Sunset" Red Maple-*Acer rubrum*
Resistant American Elm-*Ulmus americana* var. "Delaware II," "Iowa State," "Liberty," "Washington"
Swamp White Oak-*Quercus bicolor*
Willow Oak-*Quercus phellos*

Small Road (R-1)

Cherrybark Oak - *Quercus pagodaefolia*
Golden Raintree-*Kolreuteria paniculata*
"Skyline" Honeylocust-*Gleditsia triacanthos* var. "Monumentale" Sugar Maple-*Acer sacharum* var. "Palo Alto" Sweetgum-*Liquidambar styraciflua* var. Persimmon-*Diospyros virginiana*
"Reg Sunset" Red Maple-*Acer rubrum* var. "Summit" Ash -*Fraxinus pennsylvanica* var. Tupelo-*Nyssa sylvatica*

Passage and Court (C-1)

Golden Raintree-*Kolreuteria paniculata*
"Culumanre" Maple-*Acer rubrum* var. "Majestic" Honeylocust-*Gleditsia triacanthos* var. "Olmsted" Maple-*Acer platanoides* var. "Prairie Sentinel" Pond Cypress-*Taxodium ascendens*

PRIVATE LOTS

Frontage Trees

American Beech-*Fagus grandifolia*
American Lindens-*Tilia americana*
Native Maples: Black, Red, Sugar
Pyramidal Oaks: Basket, Pin, Sawtooth, Sarlet, Shingle, Willow
Shagbark Hickory-*Carya ovata*
Sourwood-*Oxydndron aboreum*
Tupelo-*Nyssa sylvatica*
White Pine-*Pinus strobus*

Yard Trees

Hickories: Bitternut, Mockernut & Shellbark
Native Oaks: Bur, Cherrybark, Chinkapin
Yellow Poplar-*Betula lutea*
Yellowwood-*Cladrastis lutea*
Yellow Buckeye-*Aesculus octandra*

VILLAGE GENERAL COMPONENT

PUBLIC TRACTS

Avenues (AV-1), (AV-2), (AV-3)
"Autumn Purple" Ash *Fraxinus americana* var. only
"Monarch of IL" Bald Cypress-*Taxodium distichum*
Red Oak-*Quercus rubra*

Large Residential Street (S-1)

"Autumn Gold" Ginko-*Ginkgo biloba* var.
"Autumn Blaze" Maple-*Acer rubrum* x *saccarinum* var.
"Brea" Lacebark Elm-*Ulmus parviflora* var.
"Emerald Queen" Maple-*Acer platanoides* var.
"Marshall's Seedless" Ash-*Fraxinus pennsylvanica* var.
"Palo Alto" Sweetgum-*Liquidambar styraciflua* var.
"Regent" Pagoda Tree-*Sophora japonica* var.
"Red Sunset" Maple-*Acer rubrum* var.

Small Street (R-1)

"Autumn Purple" Ash -*Fraxinus americana* var. cross alternating with "Bonfire" Maple-*Acer* x *saccharum*
Chinkapin Oak-*Quercus muehlenbergii* cross alternating with Shumard Oak-*Quercus shumardii*
Red Oak *Quercus rubra* cross alternating with Red Elm-*Ulmus rubra*
Resistant American Elm-*Ulmus americana* var. cross alternating with Pignut Hickory

Passage and Court (C-1)

Golden Raintree-*Kolreuteria paniculata*
"Culumanre" Maple-*Acer rubrum* var. "Majestic" Honeylocust-*Gleditsia triacanthos* var. "Olmsted" Maple-*Acer platanoides* var. "Prairie Sentinel" Pond Cypress-*Taxodium ascendens*

Alley (A-1)

Black Cherry-*Prunus serotina*
Persimmon-*Diospyros verginiana*
Red Maple-*Acer rubrum*
Sweetgum-*Liquidambar styraciflua*
Tupelo-*Nyssa sylvatica*
White Ash-*Fraxinus americana*

PRIVATE LOTS

Frontage

American Lindes-*Tilia americana*
Basket Oak-*Quercus michauxii*
"Palo Alto Sweetgum-*Liquidambar styraciflua*
Pin Oak-*Quercus palustris*
Red Maple-*Acer rubrum*
Scarlet Oak-*Quercus coccinea*
Silver Lindens-*Tilia tomentosa*
Tupelo-*Nyssa Sylvatica*

Yard

"Autumn Purple" Ash-*Fraxinus americana* var. only
"Autumn Splendor" Buckeye-*Aesculus x arnoldiana*
Cherrybark Oak-*Quercus pagodaefolia*
Chinkapin Oak-*Quercus muehlenbergii*
"Green Mountain" Maple-*Acer saccharum* x *negra*
Loblolly Pine-*Pinus taeda*
Shumard Oak-*Quercus shumardii*

VILLAGE CENTER COMPONENT

PUBLIC TRACTS

Avenues (AV-1, AV-2, AV-3, AV-4)
Sycamore-*Platanus occidentalis*

Small Commercial Street (S-1)

Shipmast Locust-*Robinia pseudo-acacia* var. *rectissima*
"Skyline" Honeylocust-*Gleditsia triacanthos* var.
Winged Elm-*Ulmus alata*

Passage and Court (C-1)

Golden Raintree-*Kolreuteria paniculata*
"Culumanre" Maple-*Acer rubrum* var. "Majestic" Honeylocust-*Gleditsia triacanthos* var. "Olmsted" Maple-*Acer platanoides* var. "Prairie Sentinel" Pond Cypress-*Taxodium ascendens*

Parking Lots

"Liberty" or "Columbia" Planetree-*Platanus occidentalis* x *orientalis* var.

PRIVATE LOTS

Frontage Trees

American Holly-*Illex oppaca*
Dogwoods: Flowering, Cornelian, Kousa, Pagoda
Littlelax Lindens-*Tilia cordata*
Magnolias: Loebner loebnerii, "Merrill" *stellata* x *kobus*, Saucer souldangiana, Sweetbay virginiana, Southern grandiflora, Star *stellata*, Yuan *hepata*
Native subcanopy trees: Fringetree *Chionanthus virginia*, Hophornbeam *Ostrya virginiana*, Redbud *Cercis canadensis*, Serviceberry *Amelanchier*, Ohio Buckeye *Aesculus glabra*, Silverbell *Halesia carolina*, Smoketree *Cotinus obavatus*

Yard Trees

"Brea" Lacebark Elm *Ulmus parviflora* var.
Black Locust *Robinia pseudo-acacia* spp. or var.
"Princeton Upright" Japanese Pagoda *Sophora japonica*
Red or Slippery Elm *Ulmus rubra*
Shademaster" Honeylocust *Gleditsia triacanthos* var.
Silverbell *Halesia carolina* or *diptera* var. *magniflora*
Winged Elm *Ulmus alata*

General Information

George W. Norton, Jr. had a great love for the land on which Norton Commons is planned. The farm was used to showcase progressive farming methods on the *WAVE-TV Farm Show*. George's daughter, Mary Shands, and her children and grandchildren share in this love of the land. Mary and her husband, Al, have been leading advocates of healthy lifestyles and clean environment through their work at the *Foxhollow Health and Wellness Center*. Triad Development Company has been an innovative, progressive developer that seeks creative and environmentally sensitive solutions to its development projects.

As the respective parties approach the development of Norton Commons, it is their desire to continue to follow the legacy of George W. Norton, Jr., and continue to use the farm for educational purposes. In this case, the parties' intent is to educate others about progressive development practices, which respect the natural environment and create neighborhoods for healthy lifestyles. Norton Commons is designed to offer these lifestyle benefits to people of all ages and a great diversity of economic income levels.

Upon approval, the Developer shall employ qualified consultants to assist in the preparation of various health and environmental guidelines for the project. Following is an outline of some of the elements the guidelines shall address:

Environmental/Healthy Neighborhood Elements

Construction Related Activities

- Soil erosion and sediment control practices
- Site planting and re-vegetation requirements
- On-site trash management system
- Site clean up requirements
- Construction waste recycling
- Construction trade education and awards program

Community Standards

Stormwater

- Stormwater management system and native plant requirements related to water quality control
- Infiltration/gray water reuse system
- Work with MSD to develop demonstration projects on site.

Landscape Management

- Homeowner guidelines for the ecology including native plant list for sustainability
- Turf management guidelines/restrictions for homeowners and lawn care companies
- Approved list of certified Norton Commons lawn care companies
- General guidelines for the maintenance of Norton Commons' community gardens, parks, squares, greens and special woodland study areas
- Norton Commons Tree Nursery for public space and private lot trees
- Trash recycling requirements

Construction and Utility Easements

- Location of easements as part of the applicable construction plans.
- Restrictions on use of heavy construction equipment in sensitive areas.
- Wildlife and Vegetative Habitat Evaluation.
- Evaluation of on-site areas for existing or potential habitat presented as a report.

Green Builder/Health House Options

- Builder incentive/awards program
- Norton Commons Learning Center on site
- Health home materials

Public Building Design Standards

- Design/material requirements for the construction of public structures within the community
- Awards program for environmentally/healthy construction of private, commercial, retail, apartment buildings and grounds

Air Quality

- Work with TARC and other agencies to seek solutions and grants to promote more opportunities for intermodal transportation within the walkable neighborhoods of Norton Commons
- Work with TARC and nearby major employers and retailers to seek opportunities to offer shuttle services
- Work with public agencies to improve the road system within the area while seeking to create roadways with good character and the ability to calm traffic

General

- Seek to establish an environmental/healthy lifestyle resource learning center in the village
- Seek to establish partnering opportunities with environmental groups, botanical garden interests, private foundations and public agencies such as the Parks Department as to better ways to use Norton Commons as a public resource
- Work with the universities and researchers to seek grants to study long term the **Smart Growth** impact of Norton Commons

Commitment

Norton Commons LLC shall develop environmental guidelines and be responsible for enforcement throughout each phase of the construction process, with the Norton Commons Community Association assuming responsibility for completed phases which have been approved by the applicable permitting agencies. The standards referenced on this page will be developed and submitted to both the Planning Commission and Citizens Advisory Committee within 90 days of receiving final project approval from the Jefferson County Fiscal Court, and upon (a) the expiration of the 30-day judicial appeal process provided no appeal or other action challenging the approval is taken, or (b) in the event an appeal or other action is filed challenging the approval, the resolution of such appeal or other action in favor of Norton Commons together with the expiration of all further time periods without further action having been taken against Norton Commons. Thereafter, the Citizens Advisory Committee shall have a 30-day period for review and comment, after which period the guidelines shall be finalized.

The environmental guidelines will be developed to be compatible with the Model Development Principles as presently published by the Site Planning Roundtable of the Center for Watershed Management, as shown on page 38(a). These Principles will be utilized to the extent that the recommended site planning approach is consistent with the Norton Commons Planned Village Development Master Plan Report and the Jefferson County Land Development Regulations. (*Note: Please see Drainage Report/Stormwater Management on page 20 of this Master Plan Report.*)

Principle 1

Design residential streets for the minimum required pavement width needed to support travel lanes, on-street parking, and emergency, maintenance, and service vehicle access. These widths should be based on traffic volume and desired speed.

Principle 2

Reduce the total length of residential streets by examining alternative street layouts to determine the best option for increasing the number of homes per unit length.

Principle 3

Wherever possible, residential street right-of-way (ROW) widths should reflect the minimum required to accommodate the travel-way, the sidewalk, and vegetated open channels. Utilities and storm drains should be located within the pavement section of the right-of-way wherever feasible. Where the right-of-way needs to be expanded to accommodate utilities or a wider street section, building setbacks may be relaxed.

Principle 4

Minimize the number of residential street cul-de-sacs and incorporate landscaped areas to reduce their impervious cover. The radius of cul-de-sacs should be the minimum required to accommodate emergency and maintenance vehicles. Alternative turnarounds should be considered.

Principle 5

Where density, topography, soils, and slope permit, vegetated open channels should be used in the street right-of-way to convey and treat stormwater runoff.

Principle 6

The required parking ratio governing a particular land use or activity should be enforced as a median of national standards in order to curb excess parking space construction. Existing parking ratios should be reviewed for conformance taking into account local and national experience to see if lower ratios are warranted and feasible.

Principle 7

Parking codes should be revised to lower parking requirements where mass transit is available or enforceable shared parking arrangements are made.

Principle 8

Reduce the overall imperviousness associated with parking lots by providing compact car spaces, minimizing stall dimensions, incorporating efficient parking lanes, and using pervious materials in the spillover parking areas where possible.

Principle 9

Wherever possible, provide stormwater treatment for parking lot runoff using bioretention areas, filter strips, and/or other practices that can be integrated into required landscaping areas and traffic islands.

Principle 10

Advocate open space development incorporating smaller lot sizes to minimize total impervious area, reduce total construction costs, conserve natural areas, provide community recreational space, and promote watershed protection.

Principle 11

Relax side yard setbacks and allow narrower frontages to reduce total road length in the community and overall site imperviousness. Relax front set back requirements to minimize driveway lengths and reduce overall lot imperviousness.

Principle 12

Promote more flexible design standards for residential subdivision sidewalks. Where practical, consider locating sidewalks on only one side of the street and providing common walkways linking pedestrian areas.

Principle 13

Reduce overall lot imperviousness by promoting alternative driveway surfaces and shared driveways that connect two or more homes together.

Principle 14

Clearly specify how community open space will be managed and designate a sustainable legal entity responsible for managing both natural and recreational open space.

Principle 15

Direct rooftop runoff to pervious areas such as yards, open channels, or vegetated areas and avoid routing rooftop runoff to the roadway and the stormwater conveyance system.

Principle 16

Create a variable width, naturally vegetated buffer system along all perennial and intermittent streams that also encompasses critical environmental features such as the 100-year floodplain, steep slopes and freshwater wetlands.

Principle 17

The riparian stream buffer should be preserved or restored with native vegetation that can be maintained throughout the plan review, delineation, construction, and occupancy stages of development.

Principle 18

Clearing and grading of forests and native vegetation at a site should be limited to the minimum amount needed to build lots, allow access, and provide fire protection. A fixed portion of any community open space should be managed as protected green space in a consolidated manner.

Principle 19

Enhance trees and other vegetation at each site by planting additional vegetation, clustering tree areas, and promoting the use of native plants. Wherever practical, manage community open space, street rights-of-way, parking lot islands, and other landscaped areas to promote natural vegetation.

Principle 20

Incentives and flexibility in the form of density compensation, buffer averaging, property tax reduction, stormwater credits, and by-right open space development should be encouraged to promote conservation of stream buffers, forests, meadows, and other areas of environmental value. In addition, off-site mitigation consistent with locally adopted watershed plans should be encouraged.

Principle 21

New stormwater outfalls should not discharge untreated or unmanaged stormwater into jurisdictional wetlands, sole-source aquifers, or other water bodies.

Principle 22

Protect drinking water supplies by restricting certain activities in wellhead areas.

Principle 23

Address the issue of development on agricultural remainder parcels outside community growth and/or priority funding areas.

WALLS

Material

- Walls shall be finished in brick, stone, cast stone, stucco, wood or Hardiplank.
- **Arches and Piers** shall be made of brick, stone, cast stone or stucco.
- **Stoops** shall be finished in wood, brick, stone, cast stone or stucco.
- **Frontage Walls** shall be stone, brick or stucco. Frontage walls may include pierced brick. Gates in garden walls shall be wood or metal.
- **Retaining Walls** shall be finished of stone, brick or stucco.
- **Fences** along frontages shall be made of wood or metal pickets. Fences at other yards may be made of closed wood boards, masonry, trellis, lattice, hedge, garage building walls or some combination thereof. Fences may have stucco, brick or stone columns.
- **Wood**, if visible, shall be painted or stained except walking surfaces, which may be left natural.

Configuration

- Walls finished in wood or Hardiplank shall be a pattern of clapboard, dropsiding or board-and-batten.
- Walls shall show no more than two materials above the basement or undercroft. Materials shall change along a horizontal line, with the heavier material below the lighter.
- Clapboard and other siding shall be painted, with a maximum of 6 inches exposed.
- Stucco shall be cement with smooth sand-finish.
- Brick mortar joints shall be struck. Courses shall be no more than 3 inches wide.
- Trim shall be a minimum of grade "B" lumber and shall not exceed 1 inch in depth or 6 inches in width at corners and around openings, except at the front door which may be any size or configuration.
- Arches and Piers of masonry shall be no less than 12" x 12".
- Posts shall be no less than 6"x 6."
- Intercolumniation on the ground floor shall have vertically proportioned openings.
- Undercrofts shall be enclosed with horizontal wood boards, wood louvers, shingles or framed wood lattice.
- Fences at side yards shall be between 3 ft and 7 ft in height.
- Colors shall be selected from the Town Architect's List.

ATTACHMENTS

Material

- Chimneys, if visible, shall be brick, stone or stucco.
- Flues may be galvanized, painted metal or terra cotta.
- Porch and loggia columns and posts shall be made of wood or cast stone. Cast metal may be used within Village Center and Village General.
- Porch Screen frames shall be made of wood.
- Decks shall be made of wood and located within rear yards only.
- Signs shall be made of painted wood or metal.
- Awnings shall be a light metal armature with a canvas membrane.
- Railings shall be generally made of wood; cast metal and wrought iron may be used within Village Center and Village General.
- Front Walks shall be brick, gravel, stone, stained concrete or a material to match the public sidewalk.

Configuration

- Chimney's, when visible, shall extend to the ground and have a projecting cap.
- Porches shall have vertically proportioned openings.
- Railings shall have horizontal top rails and vertical bottom rails centered on the balusters. Bottom rails shall clear the floor. Balusters shall have a minimum 2 inch diameter and shall not be spaced greater than 6 inches on center.
- Balconies which cantilever shall be visibly supported by brackets and shall not exceed 3 ft in depth.
- Signs attached to buildings shall be integral to the storefronts, no larger than 2 ft in height by any length, and shall be externally illuminated.
- Pedestrian Signs may be attached perpendicular to the facade extending up to 4 ft from the frontage line and shall not exceed 2 ft in height.
- Storefront Signage shall be gloss painted a unified dark background in color. Lettering may be any color.
- Awnings shall be sloping rectangles without side or bottom soffit panels. Awnings shall not be internally lit.
- Panelized Materials, including keystones and quoins, shall be permitted only by approval of the Town Architect.
- Equipment including HVAC, utility meters, clotheslines, satellite dishes, play equipment, hot tubs and the like shall be permitted at rear yards only.
- Porches must be a minimum of 8 ft in depth and may not be enclosed by glass at frontages.
- Driveways at building frontages are only allowed for properties without alley access, and shall be a maximum of 10 ft wide.
- Colors shall be selected from the Town Architect's list.

ROOFS

Material

- Roofs, if sloped, shall be clad in clay tile, galvanized metal, concrete tile, wood singles, fiberglass shingles or dimensioned asphalt. Roof colors shall be natural finish, red or green; in the Village Center, flat roofs shall be E.P.D.M. commercial type or built-up roofing.
- Gutters, downspouts and projecting drainpipes shall be made of copper, galvanized metal or painted aluminum.
- Flashing shall be made of copper, galvanized metal or painted aluminum.

Configuration

- Overlapping gables (roof ends in which a smaller gable sits in front of a larger gable) are only permitted with the smaller gable is associated with a balcony, porch or entrance.
- Principal Roofs, if sloped, shall be symmetrical gable or hip angled between 6:12 and 10:12. If flat, Principal Roofs shall be surrounded by a horizontal parapet wall no less than 3.5 ft high from the roof deck.
- Accessory Roofs (attached to walls of the principal building) may be sheds angled to no less than 3:12.
- Eaves shall be continuous. Eaves which overhang less than 1 ft shall have a closed soffit. Eaves which overhang more than 1 ft shall have exposed rafters.
- Gutters shall be profiled at closed soffits and half-round at exposed eaves.
- Rafter Tails shall not exceed 6 inches in depth at the tip.
- Dormers shall be habitable and placed a minimum of 3 ft from side building walls.
- Roof Penetrations, including vent stacks, shall be placed back away from the principal frontage of the roof and finished to match the color of the roof.
- Skylights shall be flat and mounted on the rear slope.
- Towers are allowed on all buildings and are recommended on buildings which terminate street vistas greater than 500 ft in length. Towers shall not exceed more than 15 ft above the roof ridge line, and shall not have a floor area exceeding 150 sq. ft.

OPENINGS

Material

- Windows shall be made of painted aluminum, wood or vinyl-clad. Front and rear windows shall have clear glass.
- Doors (including garage doors) shall be painted wood or composite wood.
- Storefronts shall be made of painted wood or metal.
- Shutters shall be either louvered or paneled, and made of painted wood, metal or Hardiplank.
- Solid metal security gates or solid rolldown windows are not permitted. Link or grill security devices are only permitted if installed from the inside, within the window or door frames.

Configuration

- Windows shall be single, double, triple-hung or operable casements. Windows shall be rectangular with a vertical or square proportion. Transoms shall be oriented horizontally with vertically proportioned panes of glass. Multiple windows in the same rough opening shall be separated by a 4" minimum post. The centerline of the window sash shall align within the centerline of the wall (no flush-mounted windows).
- Mullions shall be true divided panes or fixed on the interior and exterior surfaces. Panes shall be similar square or vertical proportions, throughout the building.
- Bay Windows shall have a minimum of 3 sides and shall extend to the floor inside and to the ground outside, or be supported by visible brackets.
- Storm Windows and Screens shall cover the entire window area.
- Doors (except garage doors) shall be side hinged (no sliders).
- Garage Doors facing a frontage shall be a maximum of 9 ft wide.
- Shutters shall be sized and shaped to match the associated openings.
- Sidelights shall not exceed 12 inches in width. Sidelights, transoms and fanlights shall have true divided lights.
- Doors on second-store balconies shall be french doors.
- Upper story openings shall be centered above lower story openings. Openings on gabled ends must be centered.
- Storefront windows shall be between 2 ft and 2.5 ft above ground level and shall reach to within 1 ft of first floor ceiling height. Storefront windows shall be lit at night.
- All doors and windows require at minimum a lintel, face frame and drip mold.
- Colors shall be selected from the Town Architect's List.

<p>General Description</p> <ul style="list-style-type: none"> This Code was conceived to guide the development of Norton Commons. The provisions of the Code are derived from the vernacular architectural and planning traditions of Kentucky. These have been modified to incorporate technical social and environmental progress. This Code shall be administered by Jefferson County as set forth in the Development Code of Jefferson County and the Norton Commons Community Association ("NCCA") documents. 		<p>Master Plan</p> <p>These drawings map with precision the Village Center, Village General and Village Edge of Norton Commons. These drawings show the approximate form and location of Open Spaces such as parks, squares, plazas and playgrounds, as well as the approximate trajectories of the thoroughfares.</p>	<p>Architectural Standards</p> <p>A matrix of text that specifies the materials and configurations permitted for walls, roofs, openings and attachments. The Architectural Standards are intended to produce visual compatibility among disparate building types. They relate the new buildings to the vernacular building traditions of the region, thus inheriting a suitable response to climate. Because urban quality is enhanced by architectural compatibility but is not dependent on it, the provisions of the Architectural Standards may range from liberal to strictly deterministic.</p>												
<p>General Notes</p> <ul style="list-style-type: none"> This Code applies to the design of all the private buildings and landscape of Norton Commons. Private buildings shall be processed by the Town Architect for compliance with its requirements. Public and Civic buildings shall comply with PVD District Regulation, but are exempted from the provisions of this Code; their design is to be negotiated with the Town Architect. Exceptions to this Code may be granted on the basis of unusual programmatic requirements, peculiar site constraints or architectural merit as determined by the Town Architect. When in conflict, the Kentucky Building Code shall take precedence over the provisions of this Code. 		<p>Urban Standards</p> <p>A matrix of text and drawings that regulates those aspects of private building which affects the public realm. The Urban Standards vary according to the Village Components of the Norton Commons Code. The Urban Standards define the streetscape, encouraging the provision of certain building elements which influence social behavior, such as stoops and porches. They also encourage certain building types in order to serve a full range of age, income and occupation. The Urban Standards make reference to the Frontage and Use Standards.</p>	<p>Environmental Guidelines</p> <p>A series of recommendations that overlay the Architectural Standards. These recommendations outline the desired thermal performance of buildings in terms of lighting and shading, ventilation and insulation, and finishes; define the materials and finishes of environmentally benign and health construction, and specify methods of construction and maintenance that minimize negative environmental impacts.</p>												
		<p>Frontage Standards</p> <p>An illustrated matrix referenced by the Urban Standards that describes the range of frontages available for residential and commercial construction, and assigns those frontages to the different sectors of the Regulating Plan.</p>	<p>Street Standards (Thoroughfare Standards)</p> <p>A matrix of drawings, specifications, and dimensions which assemble vehicular and pedestrian ways into sets specialized in both capacity and character. The Thoroughfares, which range from urban to rural, are assigned to appropriate locations in the Thoroughfare Plan.</p>												
<p>The Code consists of eight documents to be used in conjunction:</p>		<p>Use Standards</p>	<p>Landscape Standards</p>												
<p style="text-align: center;">MASTER PLAN MASTER PLAN/PRELIMINARY PLAN AND RECORD SUBDIVISION PLAT/FINAL PLAN</p> <table style="width: 100%; border: none;"> <tr> <td style="width: 25%; text-align: center;">Urban Standards</td> <td style="width: 25%; text-align: center;">Thoroughfare Standards</td> <td style="width: 25%; text-align: center;">Environmental Standards</td> <td style="width: 25%; text-align: center;">Landscape Standards</td> </tr> <tr> <td style="text-align: center;">Use Standards</td> <td style="text-align: center;">Frontage Standards</td> <td></td> <td></td> </tr> <tr> <td></td> <td></td> <td></td> <td style="text-align: center;">Architectural Standards</td> </tr> </table>	Urban Standards	Thoroughfare Standards	Environmental Standards	Landscape Standards	Use Standards	Frontage Standards						Architectural Standards		<p>A matrix of text referenced by the Urban Standards that designates the uses permitted in each of the Village Components. The uses include residential, lodging, office and retail, each to varying degrees, with emphasis on allowing flexibility wherever possible. Parking needs are correlated to the various combinations of use.</p>	<p>A list of plant species with instructions regarding their location, planting pattern and maintenance. The lists are separated into those pertaining to public Open Spaces and Thoroughfares and those pertaining to Private Lots at frontage yards. The Public and Private planting lists are coordinated towards achieving a visually coherent reforestation of the urban fabric. The selection and disposition of the species is intended to support the urban to rural transect character of each zone and to coalesce the private and public landscaping towards an ecosystem harmonious with the region.</p>
Urban Standards	Thoroughfare Standards	Environmental Standards	Landscape Standards												
Use Standards	Frontage Standards														
			Architectural Standards												

Use of the Park and Open Space System

The 155 acre park and open space system will be developed as a series of squares, plazas, greens and recreation areas. These areas are designated on the Master Plan Map and represent 26% of the area of the entire development. Of the planned 155 acres, 89 acres are located in the Jefferson County portion of the project with the remaining 66 acres located in Oldham County.

Development and Commitment of the Park and Open Space System

Designated park and open space areas will be developed concurrently with the residential and non-residential components of the project. This means that each land use phase, or increment, of site area committed to a Final Plan will be supported by park and open space equal to a minimum of 26% of the gross land area represented by the Final Plan. Sufficient park and open space area to satisfy this requirement may be included as part of (a) a Final Plan for a land use phase, or increment thereof, or (b) may be committed by separate Final Plan approval, or (3) by deed restriction of the minimum required park and open space within Jefferson and Oldham Counties. Each increment of the park and open space system so committed shall be designated for public access purposes, pursuant to the requirements of the PVD ordinance. However, each increment of system commitment will not be required to meet the 70% publicly accessible criteria as long as this criteria will be met when the entire 155 acre park and open space system is completed.

Since 89 acres of park and open space are included in the Jefferson County portion of the project, development of this portion of the project can proceed as long as the 26% ratio of parkland to development area is maintained. However, if any portion of the park and open space system physically located in Oldham County is necessary to maintain the 26% ratio within the Jefferson County portion of the project then one of the following additional criteria shall also be satisfied:

- (a) The Oldham County portion of the project has received all approvals from the Oldham County Planning and Zoning Commission and Oldham County Fiscal Court necessary to ensure the permanent commitment of lands within Oldham County for park and open space purposes commensurate with the requirements of the Jefferson County PVD ordinance. In addition, the Norton Commons developer, and its heirs and assigns, agrees not to change the park and open space designation of lands located in Oldham County without the prior approval of the Louisville and Jefferson County Planning Commission.

- OR -

- (b) The Norton Commons developer shall provide to the Louisville and Jefferson County Planning Commission, in a form acceptable to its legal counsel, a deed restriction for parks and open space lands within the Oldham County portion of the project. This deed restriction shall name the Louisville and Jefferson County Planning Commission as one of parties to whom the restriction is provided. The acreage of lands subject to this deed restriction shall be equal to the acreage required to maintain the 26% ratio in Jefferson County relative to any Final Plan approval.

Special Woodland Study Area

A portion of the planned park and open space system as represented on the map contained on the following page, has been designated as a Special Woodland Study Area. The purpose of this designation is to recognize the special status and function of this area as habitat, regulatory and non-regulatory wetlands and buffer. The Norton Commons developer and its successors and assigns will maintain the essential nature of this area in perpetuity. This means that the area will be maintained in a natural condition, but that public access can be permitted consistent with the intent of maintaining the habitat and stormwater function of the area. A more definite plan for public access to this area will be developed in close coordination with the Norton Commons Citizens Advisory Committee.

The boundary of the area as represented on the attached map will be considered to be preliminary until such time as the area has been included and approved as part of a Final Plan.

Major and Minor Amendments to the Parks and Open Space System

Lands that may be committed to the Park and Open Space System, which are in addition to those shown on the Master Plan Map, will not be considered as a major amendment of the Master Plan Map unless the additional lands are the basis for a request for residential or non-residential development in addition to that permitted by the Master Plan.

Lighting Standards

Street and site lighting standards are intended to minimize the potential of lights to adversely impact adjoining and nearby property owners.

Within Village Center and Village General areas, all lighting fixtures more than twelve feet in height above natural grade, whether free standing or attached, shall be directed downward and away from adjacent land uses. These lighting fixtures shall have a 90-degree cutoff and height of the light standard shall be set so that no light source is visible off-site. Light levels for individual sites within a Village Center shall not exceed two foot candles measured at the property line.

Within the Village Edge, all lighting fixtures regardless of height shall be directed downward and away from adjacent land uses. These lighting fixtures shall have a 90-degree cutoff and height of the light standard shall be set so that no light source is visible off-site. Light levels for individual sites within a Village Edge shall not exceed two foot candles measured at the property line. Property owners shall obtain certification by a qualified lighting expert prior to requesting a building permit review by the Norton Commons Town Architect. The Jefferson County Code Enforcement Division shall not issue a building permit for a site within a designated village edge without the express concurrence of the Town Architect that these standards have been satisfied.

The only permitted exception to these standards will be for lighting fixtures temporarily erected for construction purposes or during a 35 day period beginning on December 1 and continuing through January 4 of each year.

Hours of Operation

The hours of operation of non-residential land uses within the Village Center, General or Edge may be restricted by the Town Architect to minimize potential nuisances to adjoining property owners. Restriction on hours of operation may be imposed on existing uses by the Town Architect. Property owners shall comply within 30 days of receipt of notice from the Town Architect. In the alternative, such restrictions may be imposed by the Town Architect as a condition of issuance of a building permit.